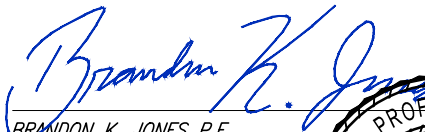
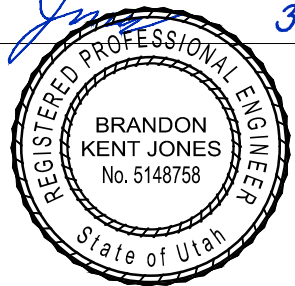


# SOUTH WEBER CITY CORPORATION

## PUBLIC WORKS STANDARD DRAWINGS


### SUBMITTED & RECOMMENDED

  
BRANDON K. JONES P.E.  
SOUTH WEBER CITY ENGINEER

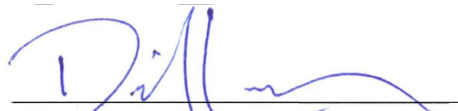


3/5/19  
DATE


### APPROVAL

  
JOLENE C. SJOBLUM  
SOUTH WEBER CITY MAYOR


3/5/19  
DATE

  
DAVID J. LARSON  
SOUTH WEBER CITY MANAGER


3/6/19  
DATE

  
BARRY BURTON  
SOUTH WEBER CITY PLANNER

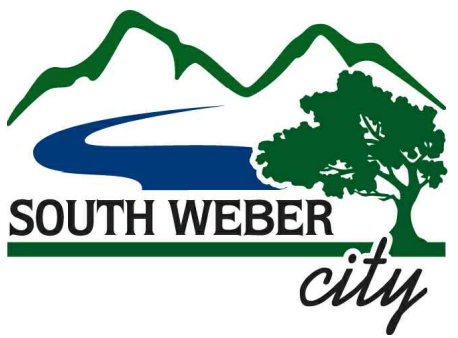
3-6-19  
DATE

  
MARK B. LARSEN  
SOUTH WEBER CITY PUBLIC WORKS DIRECTOR

3/5/19  
DATE

  
LISA SMITH  
ATTEST, SOUTH WEBER CITY RECORDER

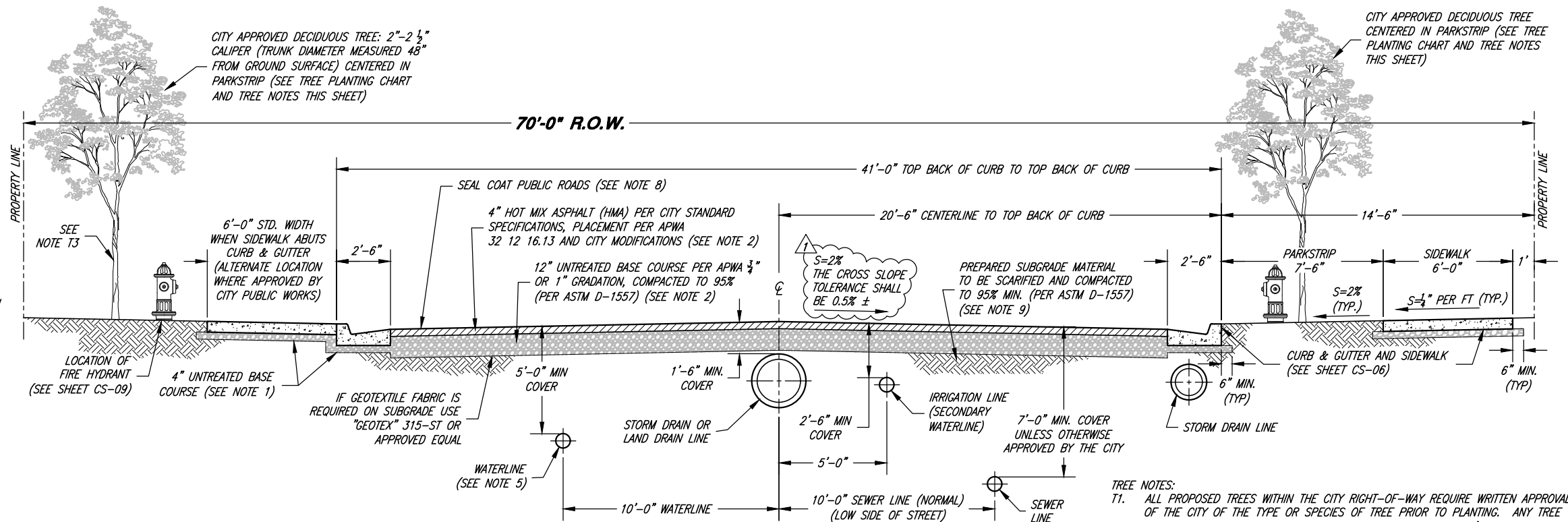
3/6/19  
DATE



### Index of Drawings

- CS-01.....TITLE PAGE & INDEX OF DRAWINGS
- CS-02.....PUBLIC ROADS - TYPICAL LOCAL STREET SECTION & UTILITY LATERAL CONFIGURATION DETAILS
- CS-03.....PUBLIC ROADS - SOUTH WEBER DRIVE & SOUTH BENCH DRIVE TYPICAL CROSS SECTION DETAILS
- CS-04.....PUBLIC ROADS - TYPICAL INTERSECTION & STREET DETAILS
- CS-05.....PUBLIC ROADS - TYPICAL DRIVE APPROACH, ASPHALT PATCH & DEFECTIVE CONCRETE REPLACEMENT DETAILS
- CS-05A...PUBLIC ROADS - APWA PLAN 255 BITUMINOUS PAVEMENT T-PATCH
- CS-06.....PUBLIC ROADS - TYPICAL ADA RAMP, SIDEWALK, CURB & GUTTER, AND CONCRETE JOINT DETAILS
- CS-07.....PUBLIC ROADS - CUL-DE-SAC & TEMP. TURNAROUND DETAILS
- CS-08.....CULINARY WATER - RESIDENTIAL WATER SERVICE DETAILS
- CS-09.....CULINARY WATER - AIR/VACUUM RELIEF STATION & FIRE HYDRANT DETAILS
- CS-10.....CULINARY WATER - TRACER WIRE INSTALLATION DETAILS
- CS-11.....CULINARY WATER - STANDARD WATER METER STATIONS
- CS-12.....CULINARY WATER - PRESSURE REDUCTION STATION
- CS-13.....CULINARY WATER - THRUST BLOCK, WATERLINE LOOP, PIPE TRENCH & MISC. VAULT DETAILS
- CS-14.....SANITARY SEWER - LATERAL & CONNECTION DETAILS
- CS-15.....SANITARY SEWER - TYPICAL MANHOLES & DETAILS
- CS-16.....STORM DRAIN - SINGLE AND DOUBLE CATCH BASIN DETAILS
- CS-17.....STORM DRAIN - DRAINAGE INLET BOX & GENERAL GRATE AND FRAME DETAILS
- CS-18.....STORM DRAIN - MANHOLE DETAILS
- CS-19.....STORM DRAIN - LARGE DETENTION BASIN DETAILS
- CS-20.....STORM DRAIN - SMALL DETENTION BASIN DETAILS
- CS-21.....GENERAL - CHAIN LINK FENCE DETAILS
- CS-22.....GENERAL - STREET LIGHTING STANDARDS
- CS-23.....GENERAL - LID (LOW IMPACT DEVELOPMENT) EXAMPLES

1. PROVIDE 4" THICKNESS OF 3/4" OR 1" UNTREATED BASE COURSE UNDER SIDEWALK, DRIVEWAY APPROACHES AND CURB & GUTTER, COMPACTED TO 95%, PER ASTM D-1557.
2. THESE PAVEMENT THICKNESS SHALL BE CONSIDERED AS CITY MINIMUMS AND MAY BE INCREASED BY THE CITY ENGINEER WHEN A GREATER DEPTH IS NECESSARY TO PROVIDE SUFFICIENT STABILITY. DESIGNER AND/OR DEVELOPER MAY SUBMIT AN ALTERNATIVE PAVEMENT DESIGN BASED ON A DETAILED SOILS ANALYSIS FOR APPROVAL BY THE CITY ENGINEER WHICH MAY MODIFY PAVEMENT THICKNESS, BUT IN NO CASE SHALL THE BITUMINOUS SURFACE COURSE BE LESS THAN 4" AND UNTREATED BASE COURSE LESS THAN 12" THICK.
3. ALL ROAD CUTS SHALL BE PATCHED PER CS-05 AND CS-13
4. CURB & GUTTER AND SIDEWALKS SHALL BE CONSTRUCTED USING FIBER REINFORCED CONCRETE AND IN COMPLIANCE WITH SOUTH WEBER CITY TECHNICAL SPECIFICATIONS AND THESE DRAWINGS.
5. ALL CULINARY WATER MAINS AND SERVICES MUST MAINTAIN A MINIMUM SEPARATION FROM ALL SEWER MAINS AND LATERALS OF 10'-0" HORIZONTAL AND 18" VERTICAL IN ACCORDANCE WITH THE STATE OF UTAH DIVISION OF DRINKING WATER RULES SECTION R309-550-7
6. THE 6'-0" SIDEWALK SHOWN ABOVE IS TO BE CONSIDERED THE "CITY STANDARD." OTHER LOCATIONS AND TYPES OF SIDEWALK AS REQUESTED BY THE DEVELOPER MUST BE APPROVED BY THE CITY. IF SIDEWALK IS LOCATED AGAINST THE TBC, IT MUST BE A MINIMUM OF 6 FEET IN WIDTH.
7. NATURAL GAS TYPICALLY LOCATED IN THE PARKSTRIP, POWER AND COMMUNICATION LINES TYPICALLY LOCATED BEHIND PROPERTY LINES OR IN LOT EASEMENTS.
8. "SEAL COAT" CONSISTS OF THE FOLLOWING:
  - a. CHIP SEAL PER APWA 32 01 13.64 AND CITY MODIFICATIONS, AND
  - b. FOG SEAL PER APWA 32 01 13.50 AND CITY MODIFICATIONS.
9. IMPORTED FILL UNDER ROADWAY SHALL BE GRANULAR BORROW 2" MAX.
10. PRIOR TO THE INSTALLATION OF PAVEMENT, THE CITY INSPECTOR MUST GIVE WRITTEN PERMISSION TO PROCEED.



**TREE NOTES:**

- T1. ALL PROPOSED TREES WITHIN THE CITY RIGHT-OF-WAY REQUIRE WRITTEN APPROVAL OF THE CITY OF THE TYPE OR SPECIES OF TREE PRIOR TO PLANTING. ANY TREE NOT APPROVED BY THE CITY SHALL BE REMOVED AT THE OWNER'S EXPENSE.**
- T2. ALL PLANTED TREES TO BE SPACED IN ACCORDANCE WITH THE SPECIES CHARACTERISTICS SUCH THAT THE TREES' CROWNS AT MATURITY WILL NOT OVERLAP WITH ANOTHER TREE NOR TOUCH OR OVERHANG A BUILDING.**
- T3. FOR ADDITIONAL HELP WITH TREE SELECTION VISIT [WWW.TREEBROWSER.ORG](http://WWW.TREEBROWSER.ORG) FOR FURTHER INFORMATION ON NATIVE AND INTRODUCED TREES GROWING IN UTAH AND THE INTERMOUNTAIN WEST.**
- T4. THE PLANTING OF TREES IN THE PARKSTRIP MAY BE A REQUIREMENT OF THE DEVELOPMENT IF DEEMED NECESSARY BY THE CITY.**

TREE PLANTING CHART				
SIZE	MATURE HEIGHT	CONCRETE OFFSET	STREET CORNER/FH	POWER/UTILITY DISTANCE
LARGE	OVER 40 FEET	8 FEET MIN. DISTANCE	30 FEET FROM STREET	10 LATERAL FEET OF ANY OVERHEAD
MEDIUM	25 FEET TO 40 FEET	6 FEET MIN. DISTANCE	CORNER & 10 FEET	UTILITY WIRE & 5 LATERAL FEET OF
SMALL	LESS THAN 25 FEET	3 FEET MIN. DISTANCE	FROM ANY FIRE HYDRANT	ANY UNDERGROUND UTILITY LINE

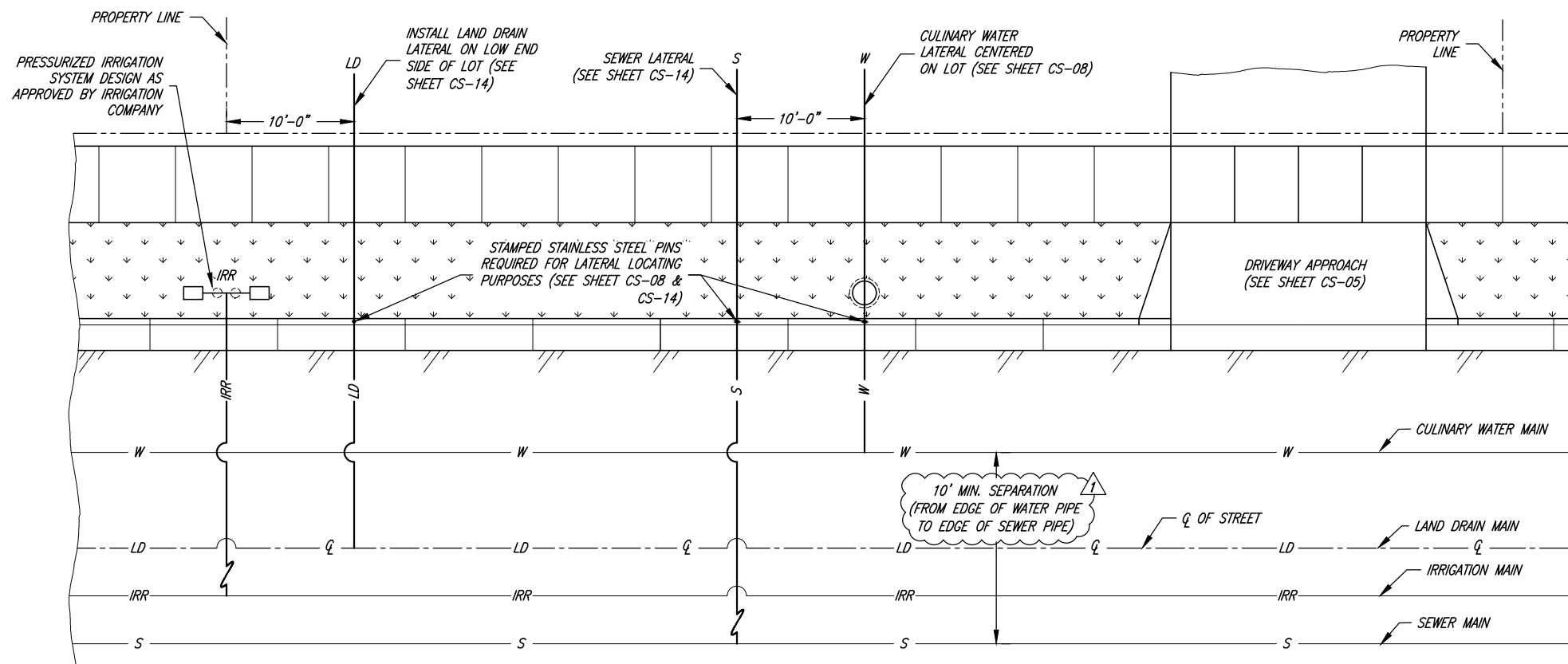



Diagram illustrating the cross-section of a road with a crowned profile. The diagram shows the top back of the curb, the difference in elevation (DIFF. IN ELEV. = 1'-0" MAX), the slope (S=2.0-4.0%), the lip to crown (LIP TO CROWN ≥ 10'-0"), and the distance from the back of the curb to the back of the curb (BACK OF CURB TO BACK OF CURB).

CROWN NOTES:

- A. MAXIMUM DIFFERENCE IN ELEVATION BETWEEN CURBS ON OPPOSITE SIDES OF THE STREET SHALL NOT EXCEED 1'-0" AS SHOWN IN DETAIL.
- B. ON CERTAIN STREETS APPROVED BY THE CITY COUNCIL, THE CITY ENGINEER WILL PROVIDE A PAVEMENT DESIGN. LOCATION OF SIDEWALK AND CURB & GUTTER MAY VARY PER DIRECTION OF THE CITY ENGINEER.
- B. ALL STREET CROSS SECTIONS SHALL BE AS APPROVED BY THE CITY ENGINEER.



  
 PROJECT ENGINEER  
 2-12-2019  
 DATE

<u>1</u>	<u>JAN '19</u>	<u>BKJ</u>	<u>ADDED AND/OR MODIFIED NOTES</u>
<u>REV.</u>	<u>DATE</u>	<u>APP.</u>	

SCALE:

N.T.S.

DRAWN BEB  
CHECKED BKJ

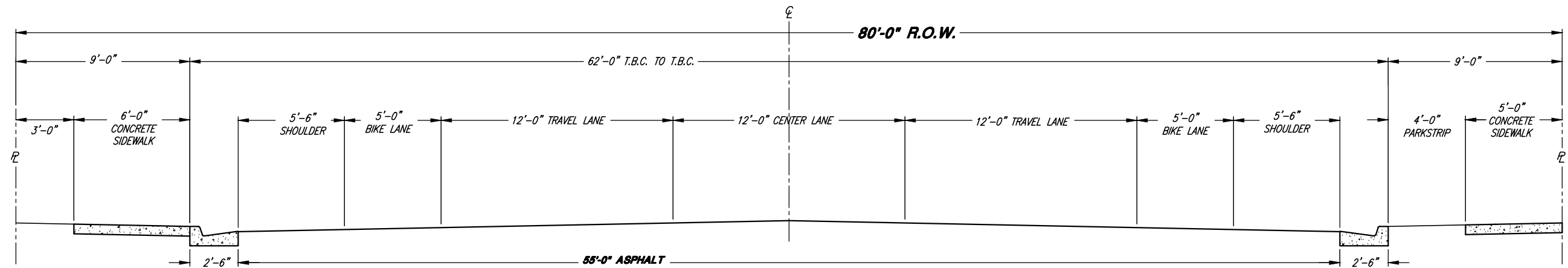


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6080 Fashion Point Drive  
South Ogden, Utah 84403 (801) 476-9767  
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***SOUTH WEBER CITY CORPORATION***  
***PUBLIC WORKS STANDARDS***

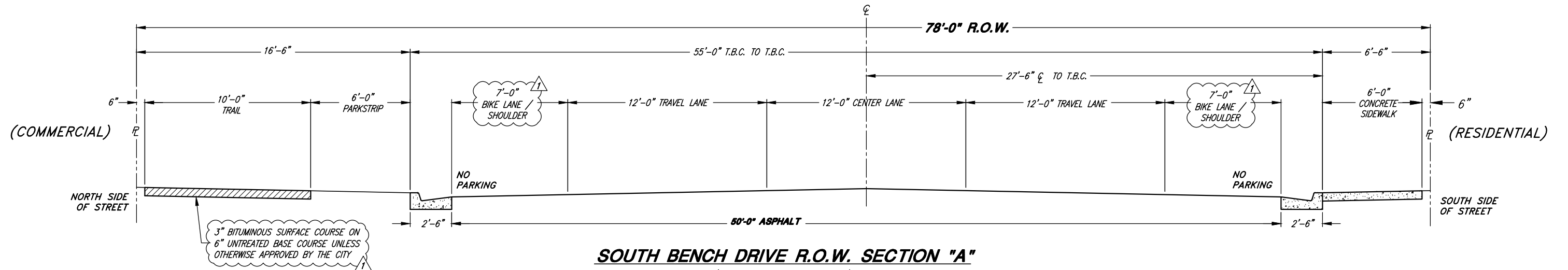
**PUBLIC ROADS - TYPICAL LOCAL STREET CROSS SECTION & UTILITY LATERAL CONFIGURATION DETAILS**

SHEET:  
**-02**  
SHEETS  
0



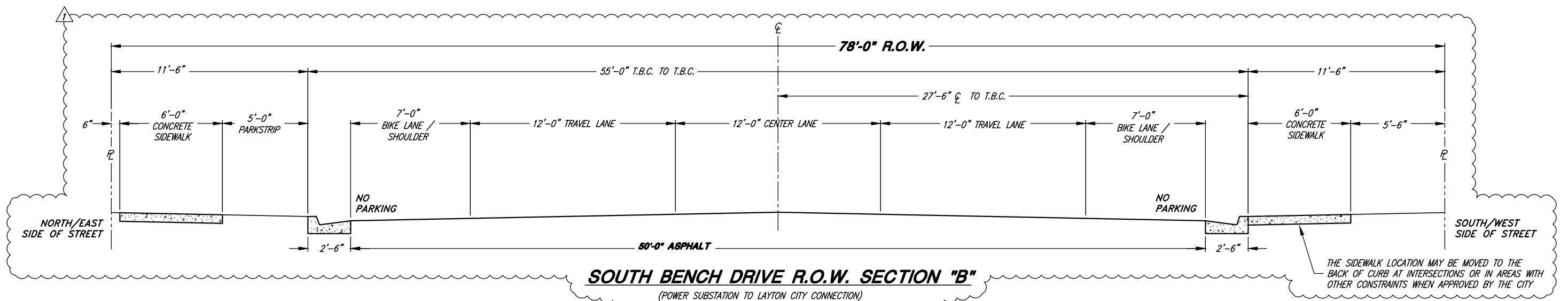
### SOUTH WEBER DRIVE R.O.W. SECTION

THE 80'-0" ROADWAY SECTION SHOWN ABOVE IS TO BE CONSIDERED THE "CITY STANDARD" FOR SOUTH WEBER DRIVE. WHERE DESIGNATED BY THE CITY ON A CASE BY CASE BASIS IN AREAS OF PRE-EXISTING ROADWAY IMPROVEMENTS, ALTERNATE STREET CROSS SECTION DESIGNS MAY BE USED WITH THE PRIOR APPROVAL OF THE CITY ENGINEER AND THE CITY PUBLIC WORKS DEPARTMENT. SUBMIT ENGINEERED CONSTRUCTION PLANS TO THE CITY ENGINEER FOR REVIEW AND WRITTEN ACCEPTANCE PRIOR TO CONSTRUCTION.



### SOUTH BENCH DRIVE R.O.W. SECTION "A"

(475 EAST TO POWER SUBSTATION)



### SOUTH BENCH DRIVE R.O.W. SECTION "B"

(POWER SUBSTATION TO LAYTON CITY CONNECTION)



BRANDON KENT JONES  
No. 5148758  
PROJECT ENGINEER  
2-12-2019  
DATE

REV.	DATE	APPR.	MODIFIED NOTES; UPDATED STREET SECTION DETAIL
1	JAN '19	BKJ	

SCALE:  
N. T.S.

DESIGNED BKJ  
DRAWN BEB  
CHECKED BKJ



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South Ogden, Utah 84403 (801) 476-9767  
www.jonescivil.com

SOUTH WEBER CITY CORPORATION  
PUBLIC WORKS STANDARDS  
PUBLIC ROADS - SOUTH WEBER DRIVE & SOUTH BENCH DRIVE  
TYPICAL ROADWAY CROSS SECTION DETAILS

SHEET:  
CS-03  
OF 24 SHEETS  
0



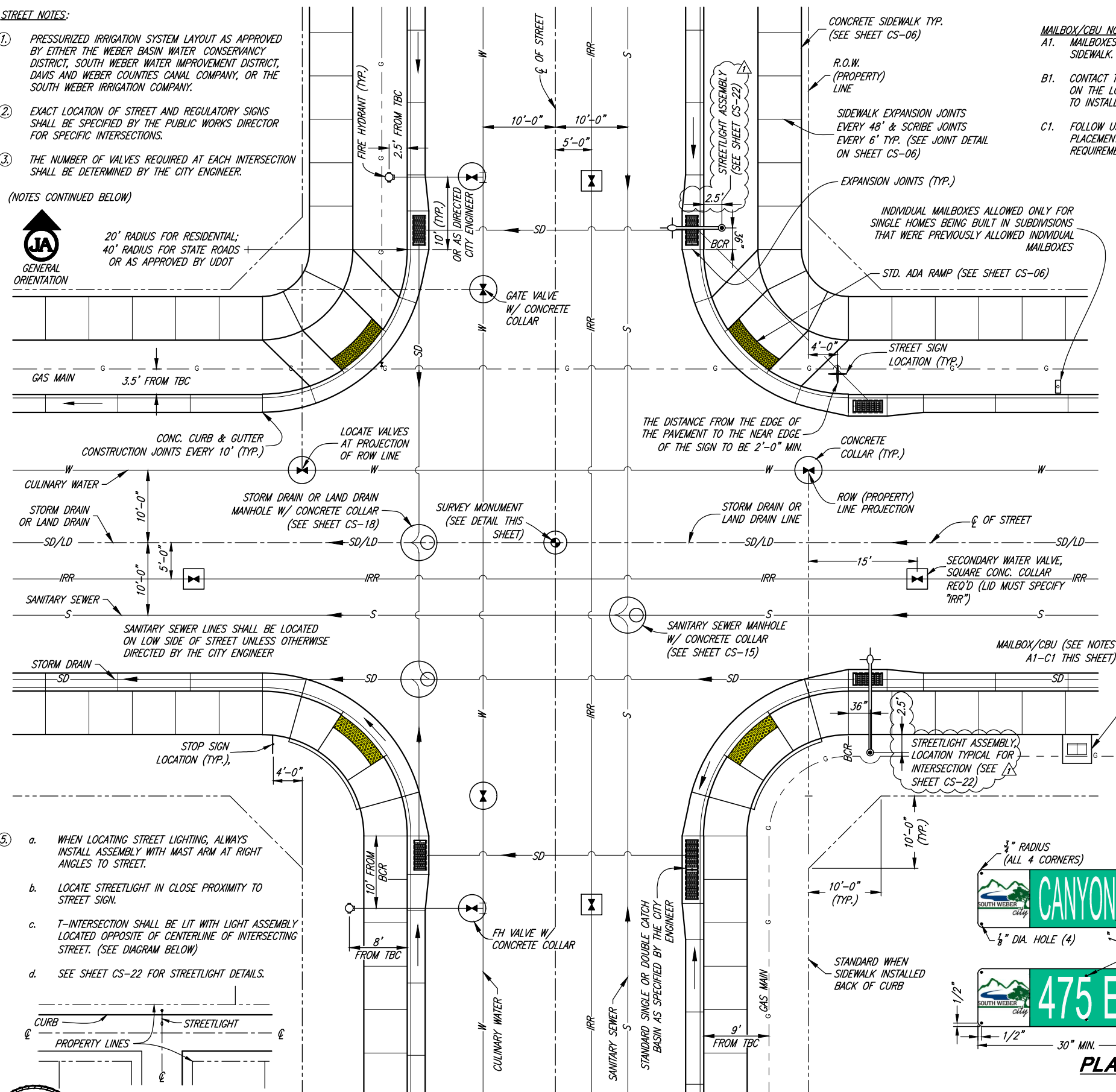
## STREET NOTES:

1. PRESSURIZED IRRIGATION SYSTEM LAYOUT AS APPROVED BY EITHER THE WEBER BASIN WATER CONSERVANCY DISTRICT, SOUTH WEBER WATER IMPROVEMENT DISTRICT, DAVIS AND WEBER COUNTIES CANAL COMPANY, OR THE SOUTH WEBER IRRIGATION COMPANY.
2. EXACT LOCATION OF STREET AND REGULATORY SIGNS SHALL BE SPECIFIED BY THE PUBLIC WORKS DIRECTOR FOR SPECIFIC INTERSECTIONS.
3. THE NUMBER OF VALVES REQUIRED AT EACH INTERSECTION SHALL BE DETERMINED BY THE CITY ENGINEER.

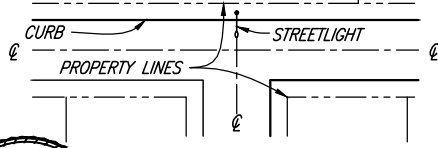
(NOTES CONTINUED BELOW)



20' RADIUS FOR RESIDENTIAL;  
40' RADIUS FOR STATE ROADS  
OR AS APPROVED BY UDOT



5. a. WHEN LOCATING STREET LIGHTING, ALWAYS INSTALL ASSEMBLY WITH MAST ARM AT RIGHT ANGLES TO STREET.
- b. LOCATE STREETLIGHT IN CLOSE PROXIMITY TO STREET SIGN.
- c. T-INTERSECTION SHALL BE LIT WITH LIGHT ASSEMBLY LOCATED OPPOSITE OF CENTERLINE OF INTERSECTING STREET. (SEE DIAGRAM BELOW)
- d. SEE SHEET CS-22 FOR STREETLIGHT DETAILS.



## MAILBOX/CBU NOTES:

- A1. MAILBOXES SHALL NOT BE PLACED IN THE SIDEWALK.
- B1. CONTACT THE LOCAL POSTMASTER FOR APPROVAL ON THE LOCATION OF THE MAILBOX OR CBU PRIOR TO INSTALLATION.
- C1. FOLLOW USPS GUIDELINES & POLICIES FOR THE PLACEMENT, INSTALLATION, AND ACCESS REQUIREMENTS FOR ALL MAILBOX AND CBU UNITS.

3" DIA. BRASS SURVEY MONUMENT  
POST - "D&L SUPPLY" K-9090

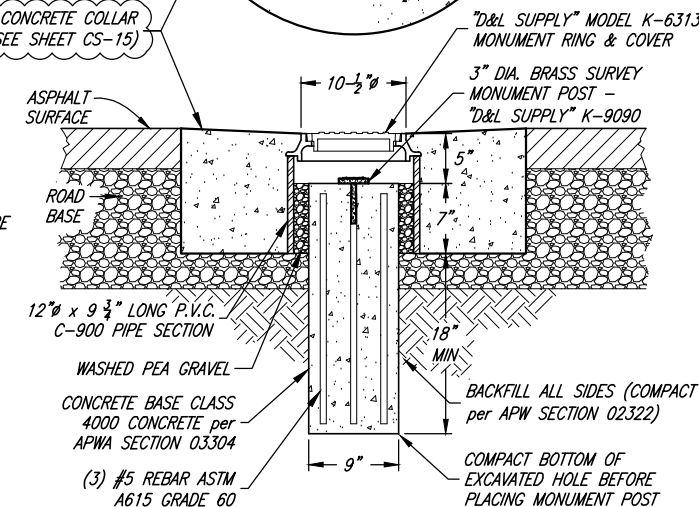
PEA GRAVEL SUMP

(3) #5 REBAR ASTM  
A615 GRADE 60

12" x 9 3/4" LONG P.V.C.  
C-900 PIPE SECTION

CONCRETE COLLAR  
(SEE SHEET CS-15)

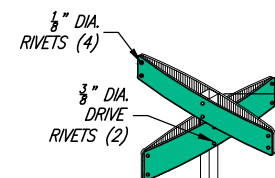
SURVEY MONUMENT NOTES:  
A2. MARK POINT WITH AN  
"X" CENTER PUNCH ON  
BRASS SURVEY MONUMENT  
CAP



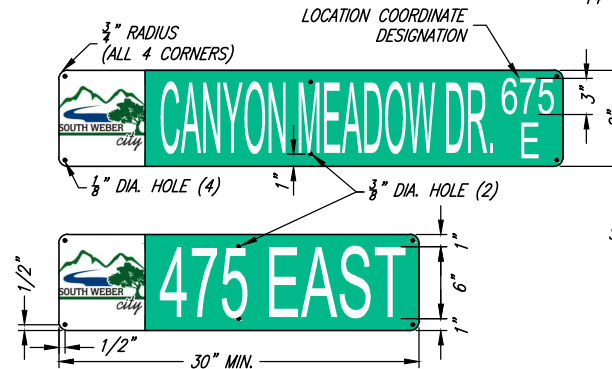
## SURVEY MONUMENT DETAIL

## STREET SIGN NOTES:

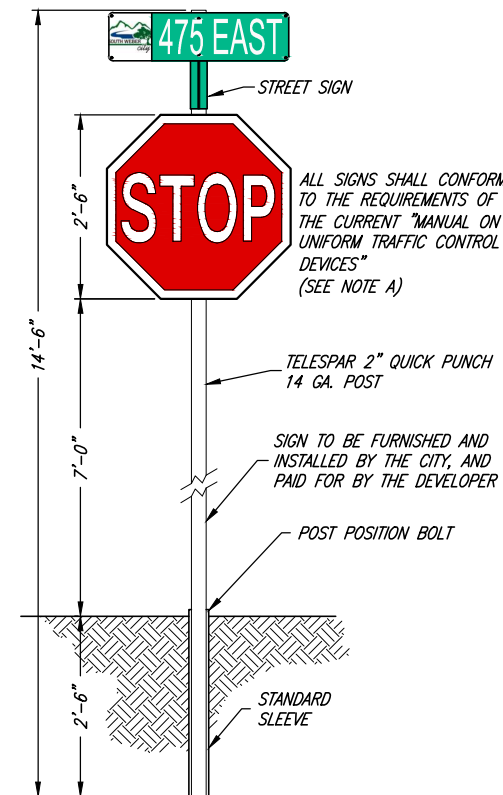
- A. STREET SIGN BACKGROUND SHALL BE REGULATORY GREEN, BOTH STREET AND TRAFFIC SIGNS SHALL BE AT THE VERY LEAST HIGH INTENSITY REFLECTIVE SHEETING (9FP-85 TYPE IIIA)
- B. LEGEND SHALL BE WHITE LETTERS, HIGH INTENSITY REFLECTIVE SHEETING (9FP-85 IIIA)
- C. SIGN BLANK SHALL BE 6081-T6 HEAT TREATED HIGH TENSILE DEGREASED ALUMINUM W/ALODINE 1200 FINISH-THICKNESS SHALL BE 0.08"
- D. EACH SIGN SHALL CONSIST OF TWO PLATES RIVETED TOGETHER & MOUNTED AS SHOWN
- E. SIGNS ON PRIVATE ROADS SHALL MEET ALL SPECIFICATIONS FOR STANDARD SIGNS, EXCEPT BACKGROUND SHALL BE BLUE.
- F. ALL STREETS WITH NAMES MUST ALSO SHOW LOCATIONS COORDINATE DESIGNATION
- G. CONTACT CITY PRIOR TO MAKING SIGNS TO VERIFY PROPER NAMES AND COORDINATES



TELESPAR 2" QUICK PUNCH  
14 GA. POST



## PLATE DETAIL

STREET SIGN  
& POSTSTREET / TRAFFIC  
SIGN & POST

BRANDON K. JONES  
PROJECT ENGINEER  
2-12-2019  
DATE

REV.	DATE	APPR.
1	JAN '19	BKJ
MODIFIED NOTES AND DIMENSIONS		

SCALE:  
N.T.S.

DESIGNED BKJ  
DRAWN BEB  
CHECKED BKJ



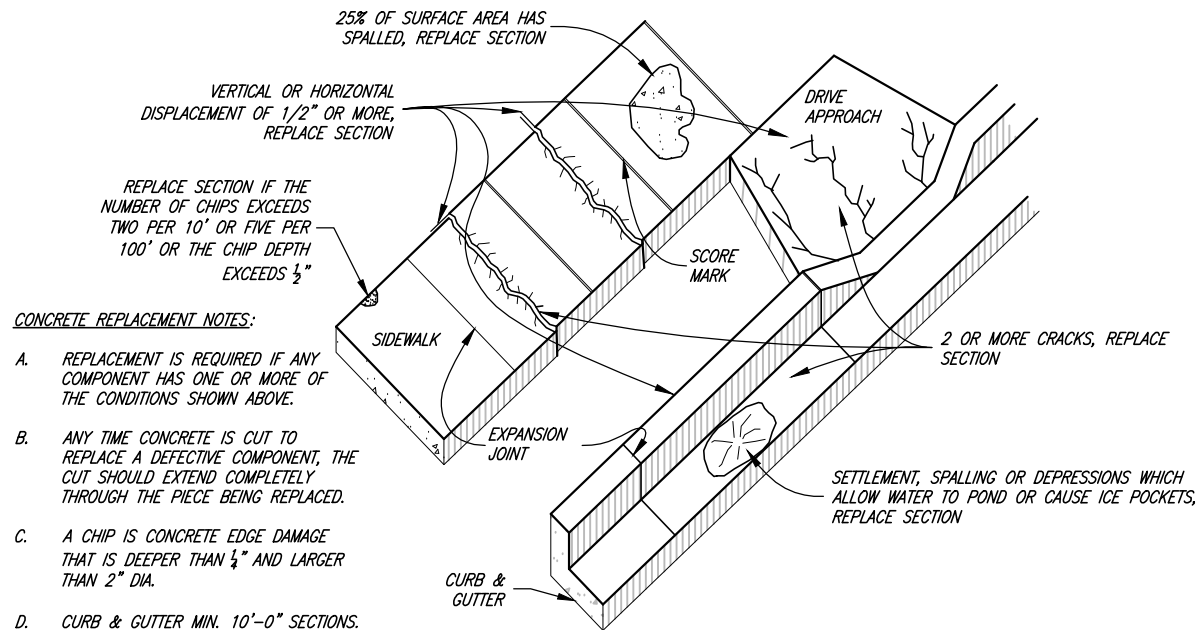
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SOUTH WEBER CITY CORPORATION  
PUBLIC WORKS STANDARDS

PUBLIC ROADS - TYPICAL INTERSECTION & STREET DETAILS

SHEET:  
**CS-04**  
OF 24 SHEETS  
0

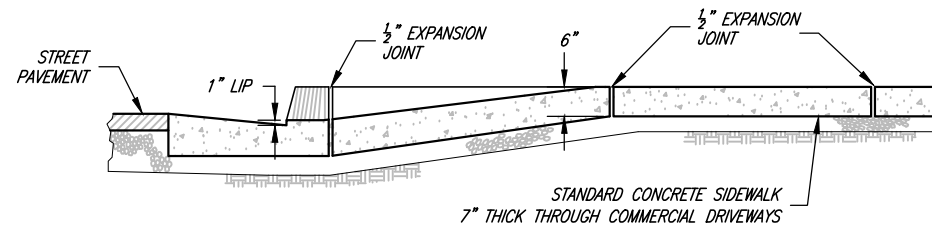
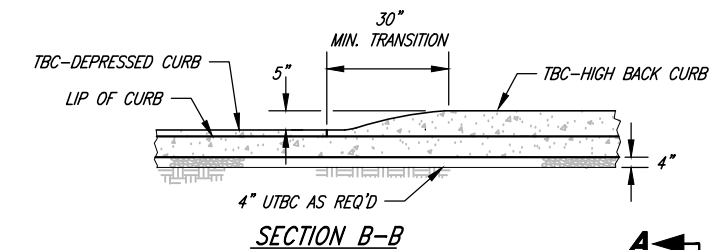




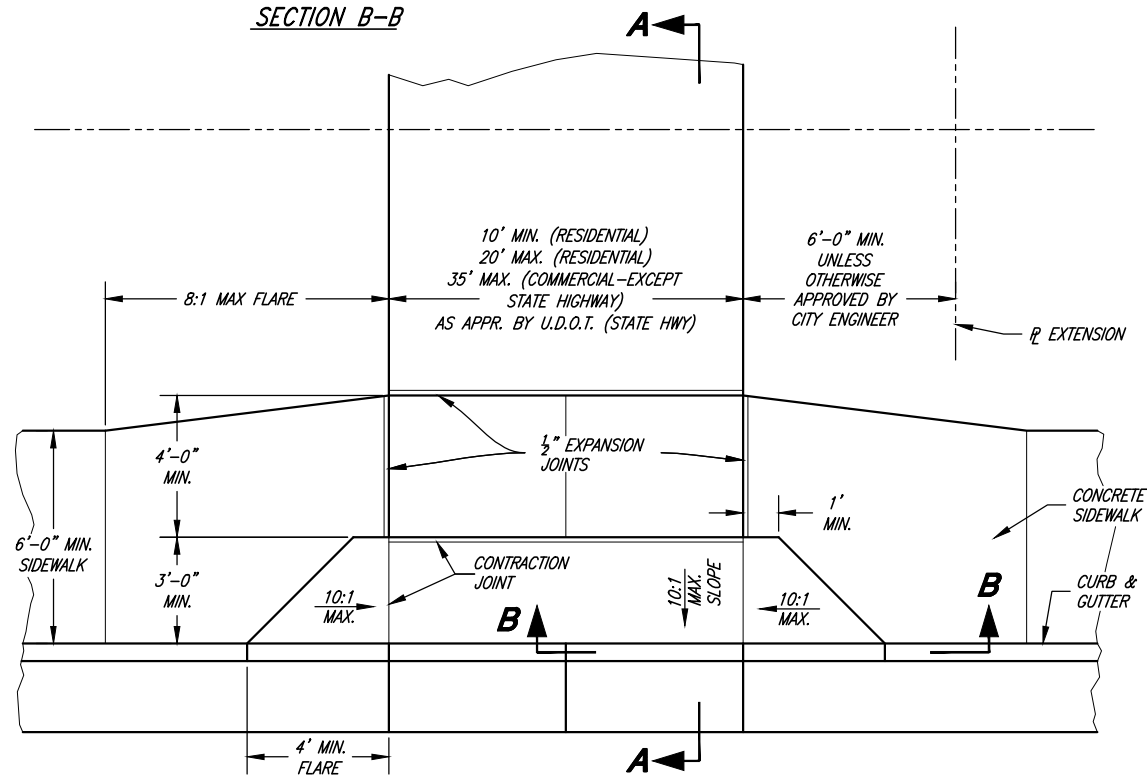
#### CONCRETE REPLACEMENT NOTES:

- REPLACEMENT IS REQUIRED IF ANY COMPONENT HAS ONE OR MORE OF THE CONDITIONS SHOWN ABOVE.
- ANY TIME CONCRETE IS CUT TO REPLACE A DEFECTIVE COMPONENT, THE CUT SHOULD EXTEND COMPLETELY THROUGH THE PIECE BEING REPLACED.
- A CHIP IS CONCRETE EDGE DAMAGE THAT IS DEEPER THAN  $\frac{1}{4}$ " AND LARGER THAN 2" DIA.
- CURB & GUTTER MIN. 10'-0" SECTIONS.

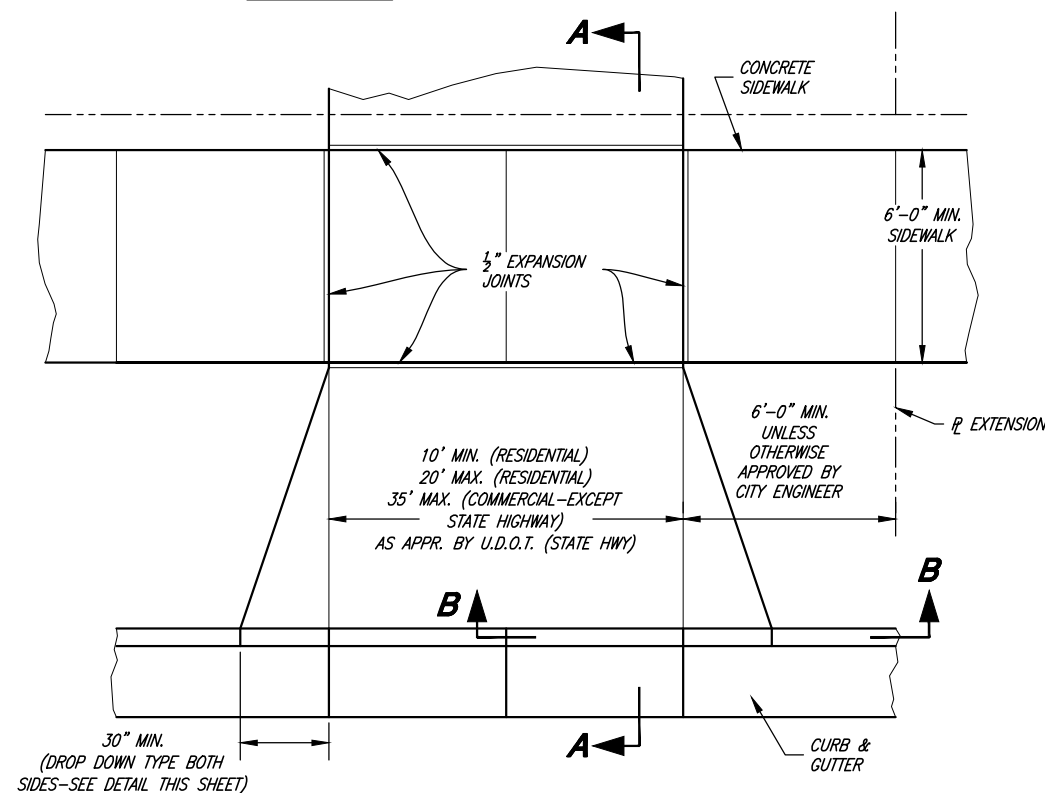
### DEFECTIVE CONCRETE REPLACEMENT CRITERIA



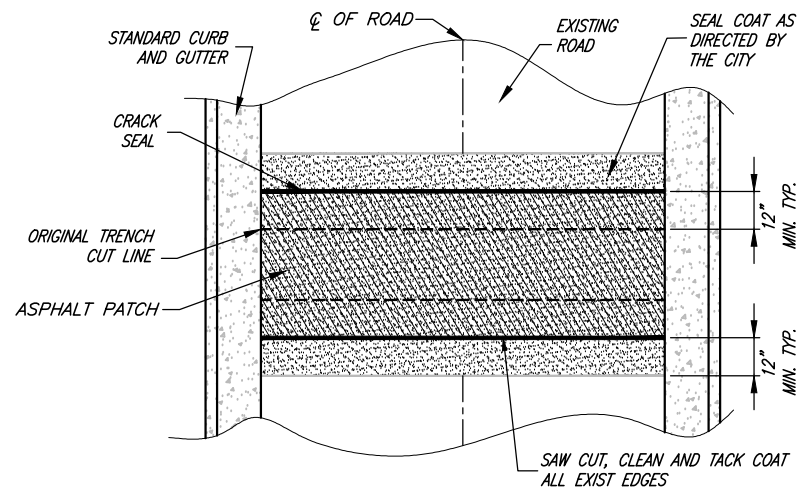
SECTION A-A



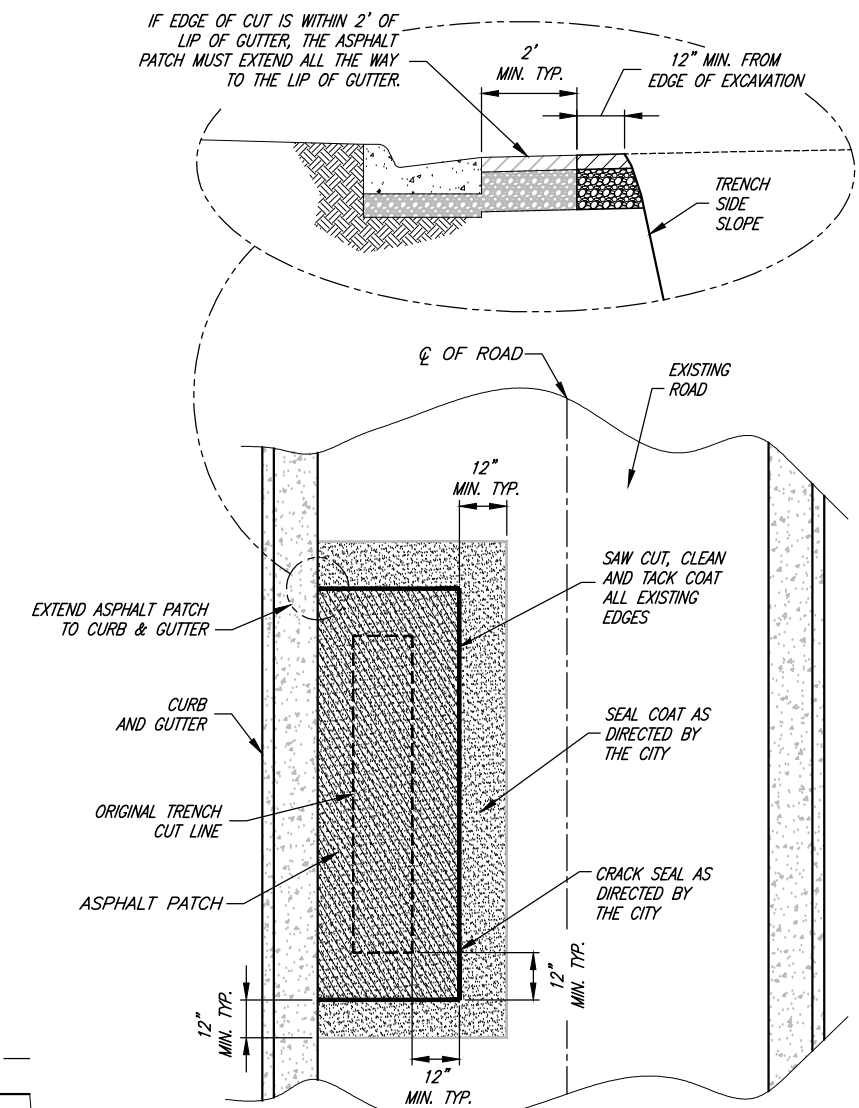
DRIVEWAY APPROACH W/ ADJACENT SIDEWALK



DRIVEWAY APPROACH W/ PARKSTRIP  
DROP DOWN STYLE (CITY STANDARD)



TYPICAL HORIZONTAL ASPHALT PATCH PLAN



TYPICAL PARALLEL ASPHALT PATCH PLAN

#### ASPHALT PATCH NOTE:

- ON ANY ROAD PAVED OR OVERLAYED WITHIN THE LAST 10 YEARS, THE PATCH MUST BE COMPLETED PER APWA PLAN 255 BITUMINOUS PAVEMENT T-PATCH. (SEE SHEET CS-05A)

#### DRIVEWAY APPROACH NOTES:

- IN NEW SUBDIVISIONS WHERE FUTURE DRIVEWAY LOCATIONS ARE UNKNOWN, THE DRIVEWAY APPROACH SHALL BE MADE BY SAW CUTTING THE BACK OF THE EXISTING CURB TO THE REQUIRED DRIVEWAY WIDTH. ALL SAW CUTTING SHALL BE ACCOMPLISHED BY A CITY APPROVED LICENSED CONTRACTOR.
- SCORE SIDEWALK  $\frac{1}{4}$ " OF SIDEWALK THICKNESS AT EACH 6'-0" SECTION. EXPANSION JOINTS AT EACH 48'-0"; PROVIDE ADDITIONAL CONTRACTION JOINTS ON OVERSIZED DRIVEWAYS AT 5'-0" MAX. SPACING



BRANDON K. JONES  
PROJECT ENGINEER  
2-12-2019  
DATE

REV.	DATE	APPR.	ADDED NOTE
1	JAN '19	BKJ	

SCALE:  
N. T.S.

DESIGNED BKJ  
DRAWN BEB  
CHECKED BKJ



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6080 Fashion Point Drive  
South Ogden, Utah 84403 (801) 476-9767  
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PUBLIC ROADS - TYPICAL DRIVE APPROACH, ASPHALT PATCH & DEFECTIVE CONCRETE REPLACEMENT DETAILS

SHEET:  
CS-05  
OF 24 SHEETS  
0

1. GENERAL:

2. *PRODUCTS:*

- ### 3. EXECUTION:

- 

AMERICAN PUBLIC WORKS ASSOCIATION  
**APWA**  
Utah Chapter

Plan  
**255**  
November 2015



*Brandon K. Inna*  
PROJECT ENGINEER  
2-12-2019  
DATE

1	JULY '19	BKJ	ADDED APWA REFERENCE NOTES & DETAILS TO STANDARDS
BKJ	DATE	APP	

SCALE:  
N.T.S.

DESIGNED BKJ  
DRAWN BEB  
CHECKED BKJ



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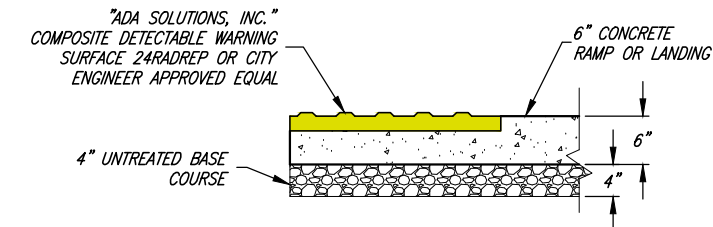
***SOUTH WEBER CITY CORPORATION***  
***PUBLIC WORKS STANDARDS***

**PUBLIC ROADS - APWA PLAN 255 BITUMINOUS PAVEMENT T-PATCH**

SHEET:  
**C-05A**  
24 SHEETS  
0

DETECTABLE WARNING SURFACE NOTES:

1. LOCATE THE DETECTABLE WARNING SURFACE SO THE OUTSIDE CORNER NEAREST THE STREET IS WITHIN 1 INCH OF THE BACK OF CURB (TBC). PROVIDE 2'-FOOT MINIMUM DEPTH.
2. PROVIDE DETECTABLE WARNING SURFACE FOR FULL WIDTH OF CURB CUT.
3. THE DETECTABLE WARNING SURFACE DOMES SHALL BE ORIENTED SUCH THAT THE ROWS ARE PARALLEL WITH THE DIRECTION OF PEDESTRIAN TRAVEL TO THE RAMP ON THE OPPOSITE SIDE OF THE STREET.
4. THE STANDARD COLOR FOR THE DETECTABLE WARNING SURFACE SHALL BE YELLOW OR PRE-APPROVED CONTRASTING COLOR. WHEN THE EXISTING SIDEWALK COLOR IS NOT STANDARD CONCRETE, THE COLOR OF THE DETECTABLE WARNING SURFACE SHALL BE DETERMINED BY THE CITY ENGINEER OR AUTHORIZED REPRESENTATIVE.
5. WHEN A DETECTABLE WARNING SURFACE DOME IS CUT, THE REMAINING PORTION OF THE DOME SHALL BE BEVELED TO A MAXIMUM SLOPE OF 1:2.



DETECTABLE WARNING SURFACE DETAIL

ADA RAMP NOTES:

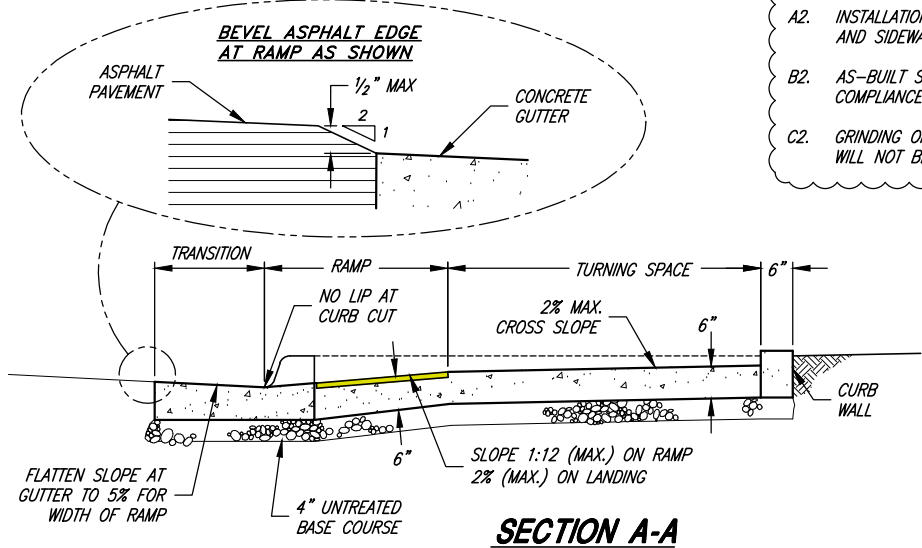
- A. WHERE DESIGNATED BY THE CITY, ALTERNATE UDOT OR APWA RAMP DESIGNS MAY BE USED WITH THE PRIOR APPROVAL OF THE CITY ENGINEER AND THE CITY PUBLIC WORKS DEPARTMENT. SUBMIT ENGINEERED CONSTRUCTION PLANS TO CITY ENGINEER FOR REVIEW AND ACCEPTANCE PRIOR TO CONSTRUCTION.
- B. SITE CONDITIONS WILL VARY. CONFIGURATION OF RAMP, LANDING, AND TRANSITION MAY BE CHANGED, BUT THEY MUST MEET DIMENSIONS AND SLOPES AS SHOWN IN THE MOST RECENT EDITION OF THE U.D.O.T. STANDARDS & SPECIFICATIONS (SHEETS PA1 THROUGH PA5). THE USE OF FLARES, CURB WALLS, ETC. ARE AT THE DISCRETION OF THE ENGINEER.
- C. LOCATE CURB CUT WITHIN CROSSWALK.
- D. RAMP GRADE BREAK MUST BE PERPENDICULAR TO THE RUNNING SLOPE.

SLOPE TABLE			
	ITEM	MAX. RUNNING SLOPE*	MAX. CROSS SLOPE*
(T)	TURNING SPACE <sup>2</sup>	2% (1V:48H)	2% (1V:48H)
(R)	RAMP	8.3% (1V:12H)	2% (1V:48H)
(S)	SIDEWALK	5% (1:20) <sup>1</sup>	2% (1V:48H)
(F1)	TRAVERSABLE SURFACE	10% (1V:10H)	--
(F2)	NON-TRAVERSABLE SURFACE	25% (1V:4H)	--
(B)	BLENDED TRANSITION	5% (1V:20H) 2% MIN.	2% (1V:48H)

\* RUNNING SLOPE IS IN THE DIRECTION OF PEDESTRIAN TRAVEL. CROSS SLOPE IS PERPENDICULAR TO PEDESTRIAN TRAVEL.

<sup>1</sup> 5% MAX OR NATURAL SLOPE OF LAND

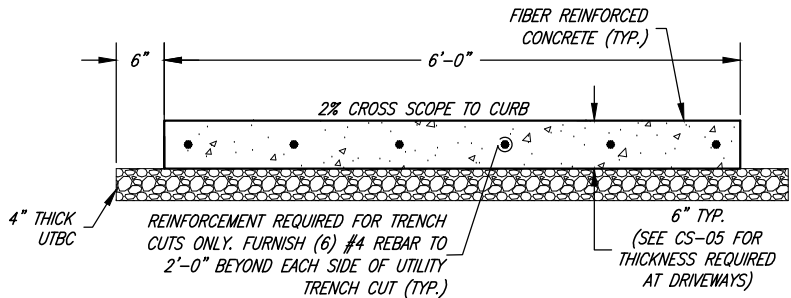
<sup>2</sup> NOT TO EXCEED 2% IN ANY DIRECTION



SECTION A-A

GENERAL NOTES:

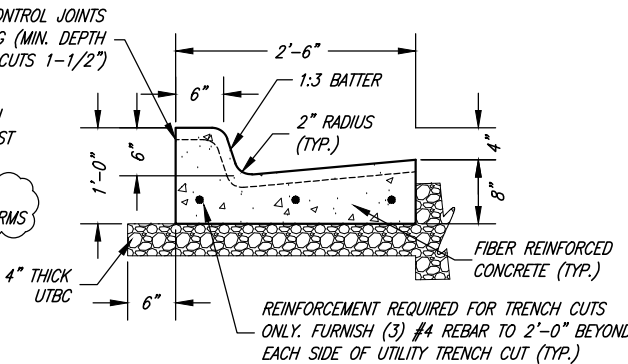
- A2. INSTALLATION TOLERANCES ON CURB & GUTTER AND SIDEWALK PER APWA 32 16 13, 3.7.
- B2. AS-BUILT SURVEY MAY BE REQUIRED TO VERIFY COMPLIANCE WITH TOLERANCES.
- C2. GRINDING OF CONCRETE, TO MEET TOLERANCES, WILL NOT BE ALLOWED.



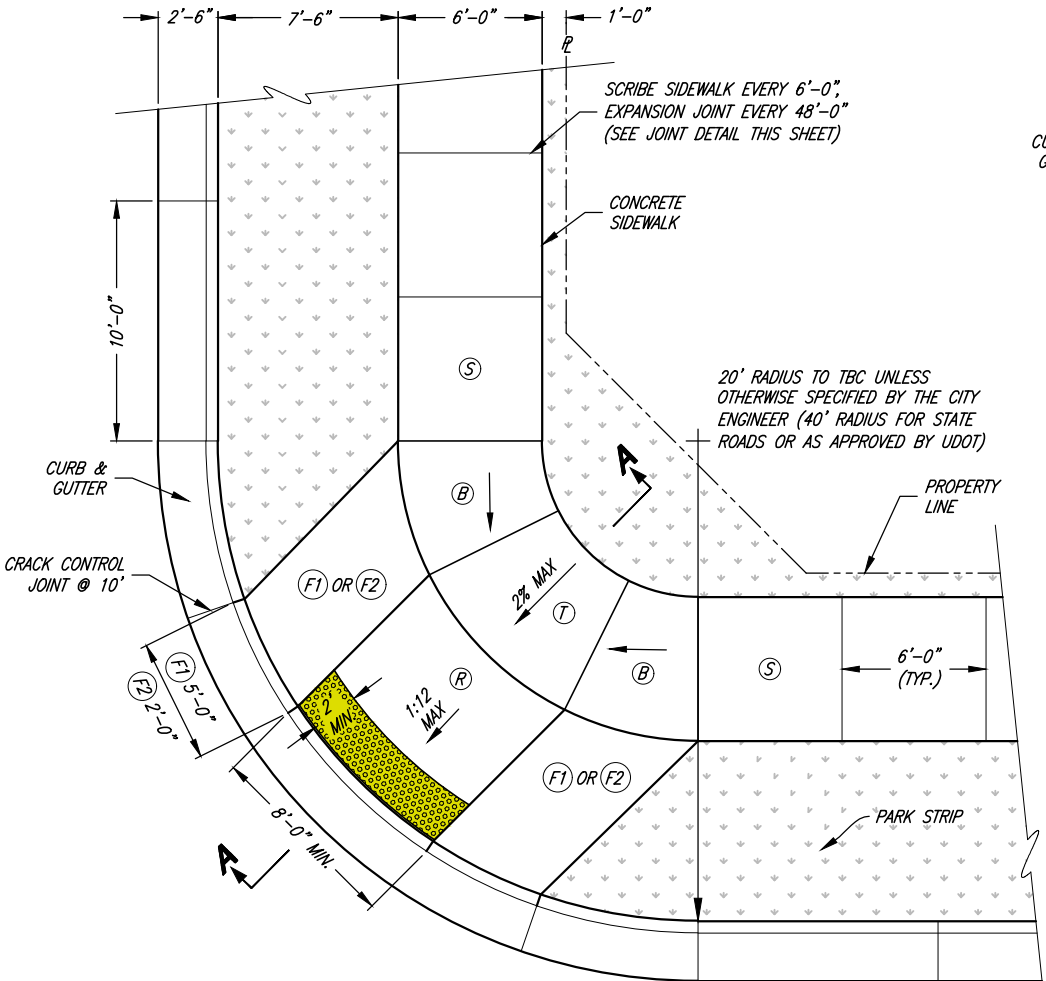
SIDEWALK SECTION  
(CITY STANDARD)

CURB & GUTTER NOTES:

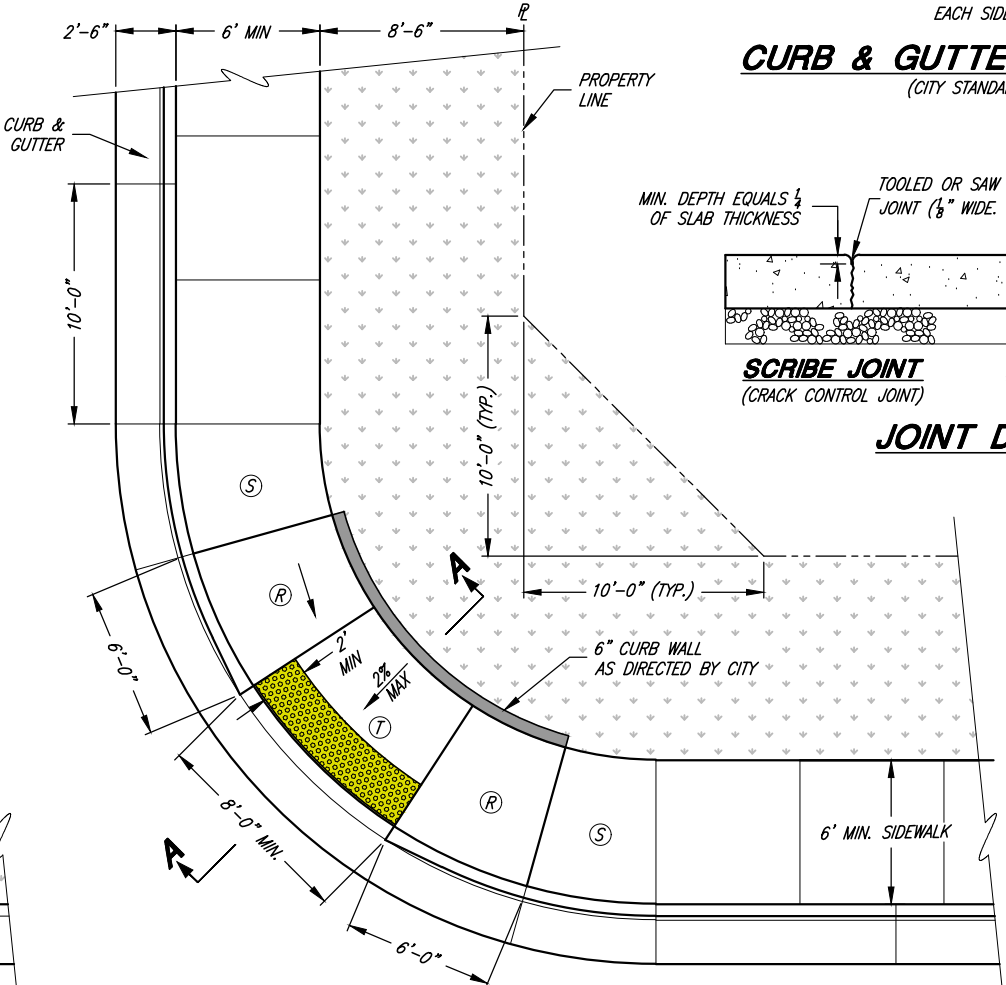
- A1. WHEN REPLACING CURB DUE TO CONSTRUCTION ACTIVITY, NEW CURB MUST EXTEND 5' MIN. PAST TRENCH ON EACH SIDE.
- B1. CONCRETE CURB TO BE CONSTRUCTED USING SLIPFORMS, HAND FORMED OR STATIONARY FORMS ARE ONLY ALLOWED FOR CURB TIE-INS.
- C1. THE SLOPE FOR CURB & GUTTER MUST BE A MINIMUM OF 0.5%.



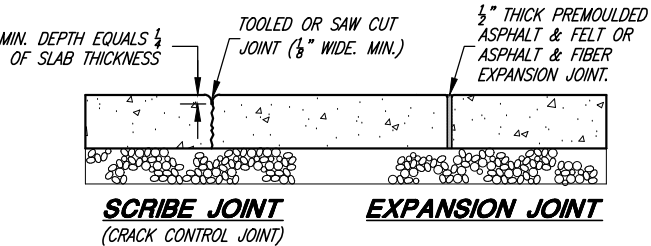
CURB & GUTTER SECTION  
(CITY STANDARD)



TYPICAL ADA RAMP  
(WITH PARK STRIP)



ADA RAMP-DETAIL "B"  
(WITHOUT PARK STRIP)



JOINT DETAIL



BRANDON K. JONES  
PROJECT ENGINEER  
2-12-2019  
DATE

REV.	DATE	APPR.	ADDED NOTES
1	JAN '19	BKJ	

SCALE:  
N.T.S.

DESIGNED BKJ  
DRAWN BEB  
CHECKED BKJ



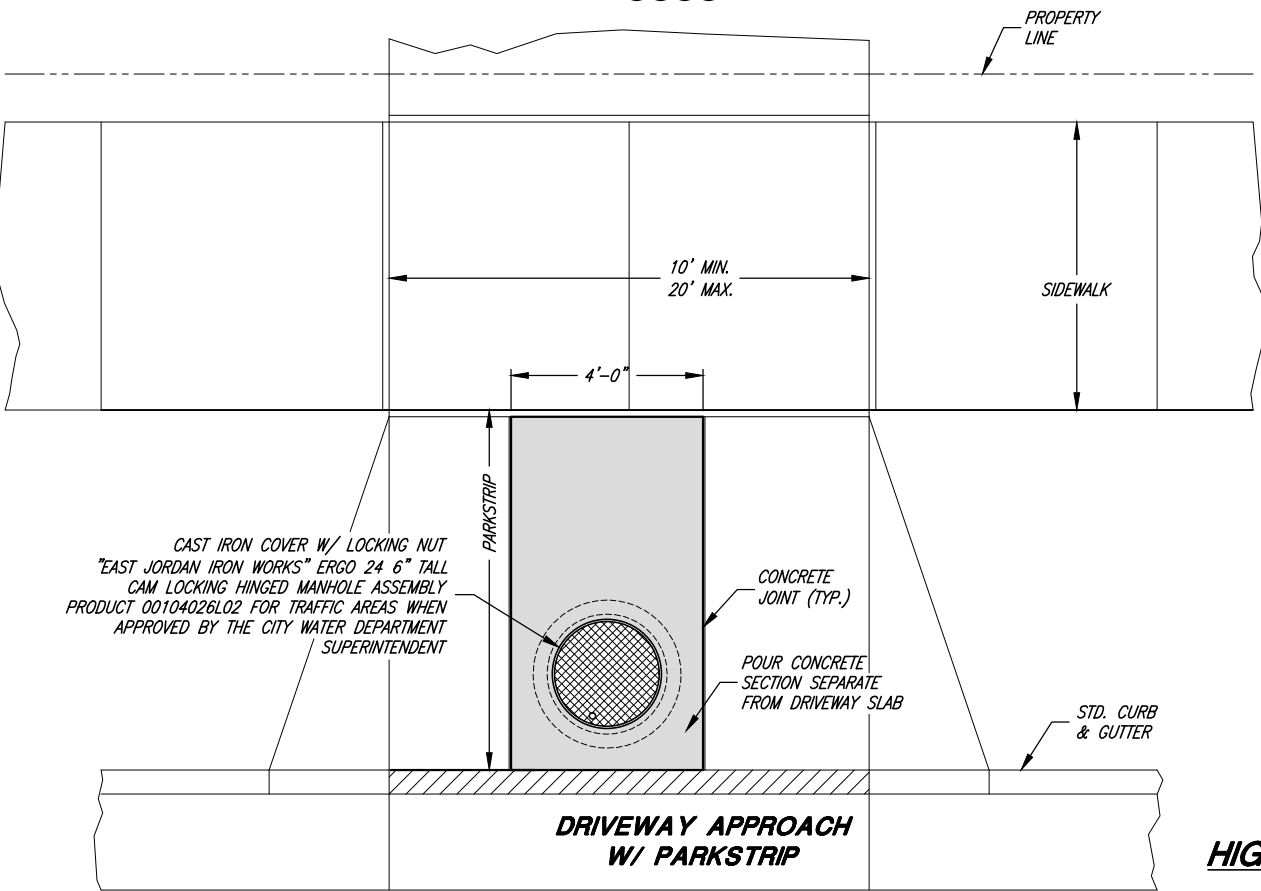
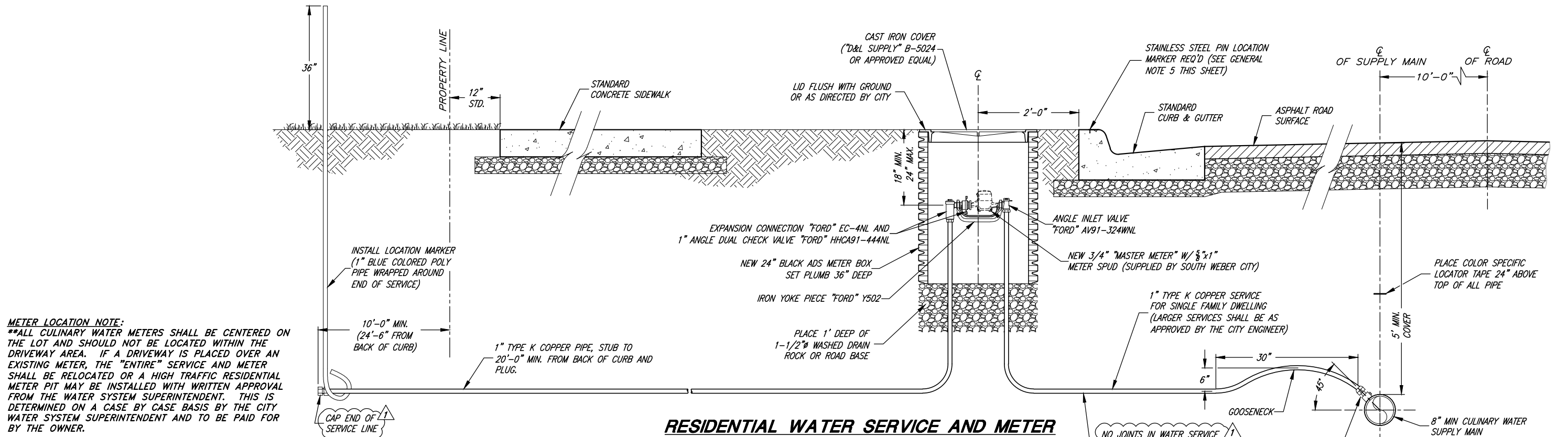
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SOUTH WEBER CITY CORPORATION  
PUBLIC WORKS STANDARDS  
PUBLIC ROADS - TYPICAL ADA RAMP, SIDEWALK, CURB & GUTTER,  
AND CONCRETE JOINT DETAILS

SHEET:  
CS-06  
OF 24 SHEETS  
0



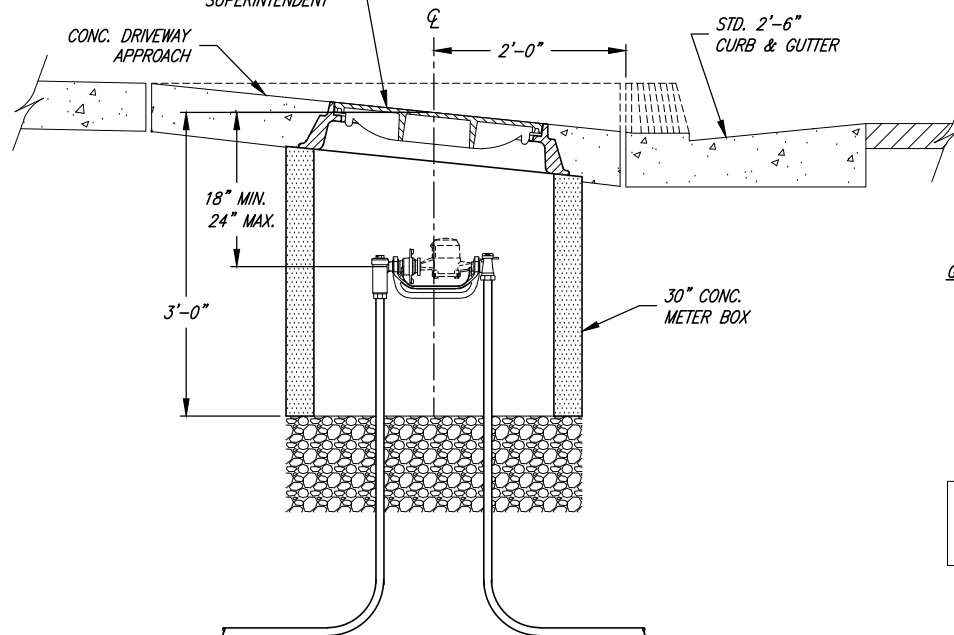




## RESIDENTIAL WATER SERVICE AND METER

CITY STANDARD

CAST IRON COVER W/ LOCKING NUT  
 "EAST JORDAN IRON WORKS" ERGO 24 6" TALL  
 CAM LOCKING HINGED MANHOLE ASSEMBLY  
 PRODUCT 00104026L02 FOR TRAFFIC AREAS WHEN  
 APPROVED BY THE CITY WATER DEPARTMENT  
 SUPERINTENDENT



## HIGH TRAFFIC RESIDENTIAL METER PIT DETAIL

THE USE OF A HIGH TRAFFIC RESIDENTIAL SERVICE METER PIT IS SITE SPECIFIC AND  
 REQUIRES WRITTEN APPROVAL FROM THE WATER SYSTEM SUPERINTENDENT PRIOR TO  
 INSTALLATION

### GENERAL NOTES:

- ALL FITTINGS SHALL BE "MUELLER" COMPRESSION TYPE UNLESS OTHERWISE NOTED.
- "BLUE" BOLTS AND NUTS ARE REQUIRED BY THE CITY.
- ALL SUPPLIES, LABOR, MACHINERY, ETC. WILL BE SUPPLIED BY THE CONTRACTOR. SOUTH WEBER CITY WILL SUPPLY AND SET THE METER ONLY ON 1" CONNECTIONS. THE CONTRACTOR SHALL SUPPLY METERS FOR CONNECTIONS GREATER THAN 1" (SEE SHEET CS-11).
- ALL SPECIFIED BRANDS OF MATERIALS SHOWN ON THESE DRAWINGS ARE "CITY STANDARDS." OTHER EQUIVALENT BRANDS MAY BE USED WITH THE PRIOR APPROVAL OF THE CITY ENGINEER AND THE CITY WATER SYSTEM SUPERINTENDENT.
- STAMPED STAINLESS STEEL PINS USED FOR LATERAL LOCATING ARE REQUIRED BY THE CITY. BLANK S.S. PINS SHALL BE PROVIDED BY THE CITY AND INSTALLED AND STAMPED BY THE CONTRACTOR DURING ALL NEW CONSTRUCTION OR RESTORED WHEN REPLACING DAMAGED CURB & GUTTER DUE TO ANY CONSTRUCTION RELATED ACTIVITY. S.S. PINS SHALL BE STAMPED "S" FOR SANITARY SEWER, "W" FOR CULINARY WATER, AND "L" FOR LAND DRAIN.



BRANDON KENT JONES  
 No. 5148758  
 PROJECT ENGINEER  
 2-12-2019  
 DATE

REV.	DATE	APPR.	ADDED NOTES
1	JAN '19	BKJ	

SCALE:  
 N. T. S.

DESIGNED BKJ  
 DRAWN BEB  
 CHECKED BKJ

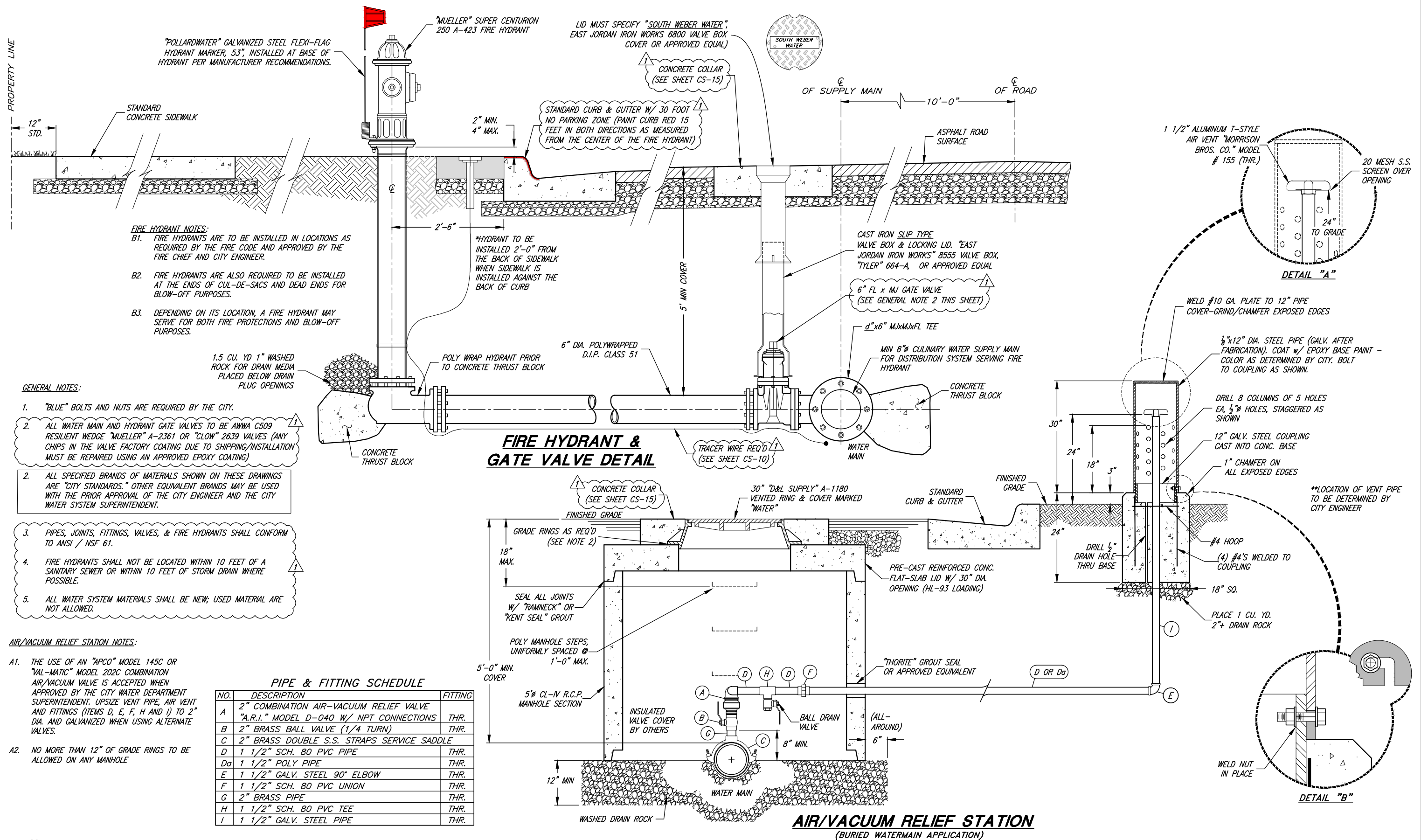


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SOUTH WEBER CITY CORPORATION  
 PUBLIC WORKS STANDARDS

CULINARY WATER - RESIDENTIAL WATER SERVICE DETAILS

SHEET:  
**CS-08**  
 OF 24 SHEETS  
 0



BRANDON KENT JONES  
No. 5148758  
PROJECT ENGINEER  
2-12-2019  
DATE

1	12/29/2017	BKJ	UPDATED FIRE HYDRANT MARKER TO FLEXI-FLAG
1	JAN '19	BKJ	ADDED AND/OR MODIFIED NOTES
REV.	DATE	APPR.	

SCALE:  
N.T.S.

DESIGNED BKJ  
DRAWN BEB  
CHECKED BKJ

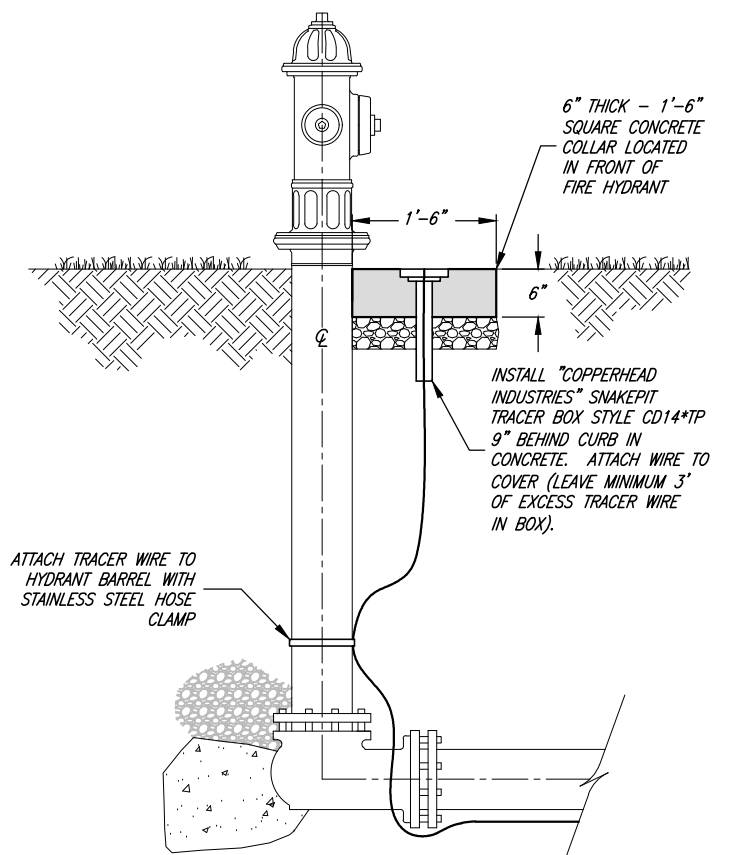
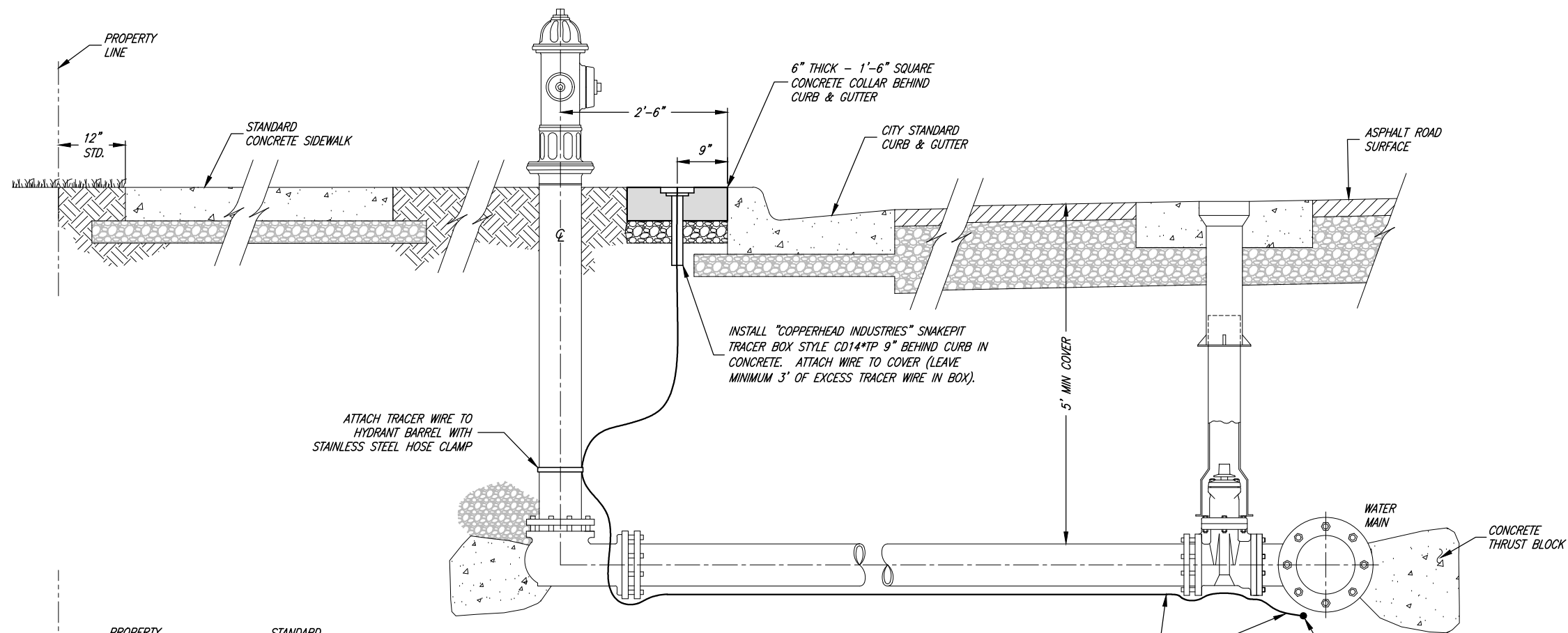


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**SOUTH WEBER CITY CORPORATION**  
**PUBLIC WORKS STANDARDS**  
**CULINARY WATER - AIR/VACUUM RELIEF STATION**  
**& FIRE HYDRANT DETAILS**

SHEET:  
**CS-09**  
OF 24 SHEETS  
0





## ALTERNATE TRACER WIRE INSTALLATION

WHERE APPROVED BY THE WATER SYSTEM SUPERINTENDENT ON STREET SECTIONS WITH NO CURB & GUTTER

### NOTES:

1. ALL WATERLINES SHALL HAVE A MINIMUM 12 GA. INSULATED TRACER WIRE INSTALLED UNDER THE HAUNCHES OF THE PIPE PRIOR TO BACKFILLING.
2. TRACER WIRES SHALL TERMINATE AT ALL FIRE HYDRANTS. AT SERVICE SADDLES AND TAPPING SLEEVES, THE TRACER WIRE SHALL NOT BE ALLOWED TO BE PLACED BETWEEN THE SADDLE AND THE PIPE. A GROUNDING ROD SHALL BE INSTALLED AT ALL TRACER SYSTEM TERMINAL POINTS.
3. TRACER WIRE SHALL BE COPPER WIRE WITH BLUE INSULATION RATED FOR DIRECT BURIAL. ALL WIRE CONNECTORS SHALL BE 3M DBR DIRECT BURY SPLICE OR PRE-APPROVED ACCEPTABLE EQUAL AND SHALL BE WATERTIGHT TO PROVIDE ELECTRICAL CONTINUITY.
4. ALL TRACER WIRE SHALL BE TESTED FOR CONTINUITY IN THE PRESENCE OF THE PUBLIC WORKS INSPECTOR PRIOR TO ASPHALT PLACEMENT. ANY TRACER WIRE FOUND NOT TO BE CONTINUOUS AFTER TESTING SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR PRIOR TO ASPHALT PLACEMENT.

## TRACER WIRE INSTALLATION

CITY STANDARD STREET SECTION (CURB & GUTTER)



PROJECT ENGINEER  
2-12-2019  
DATE

REV.	DATE	APPR.
1	JAN '19	BKJ

SCALE:

N. T.S.

DESIGNED BKJ  
DRAWN BEB  
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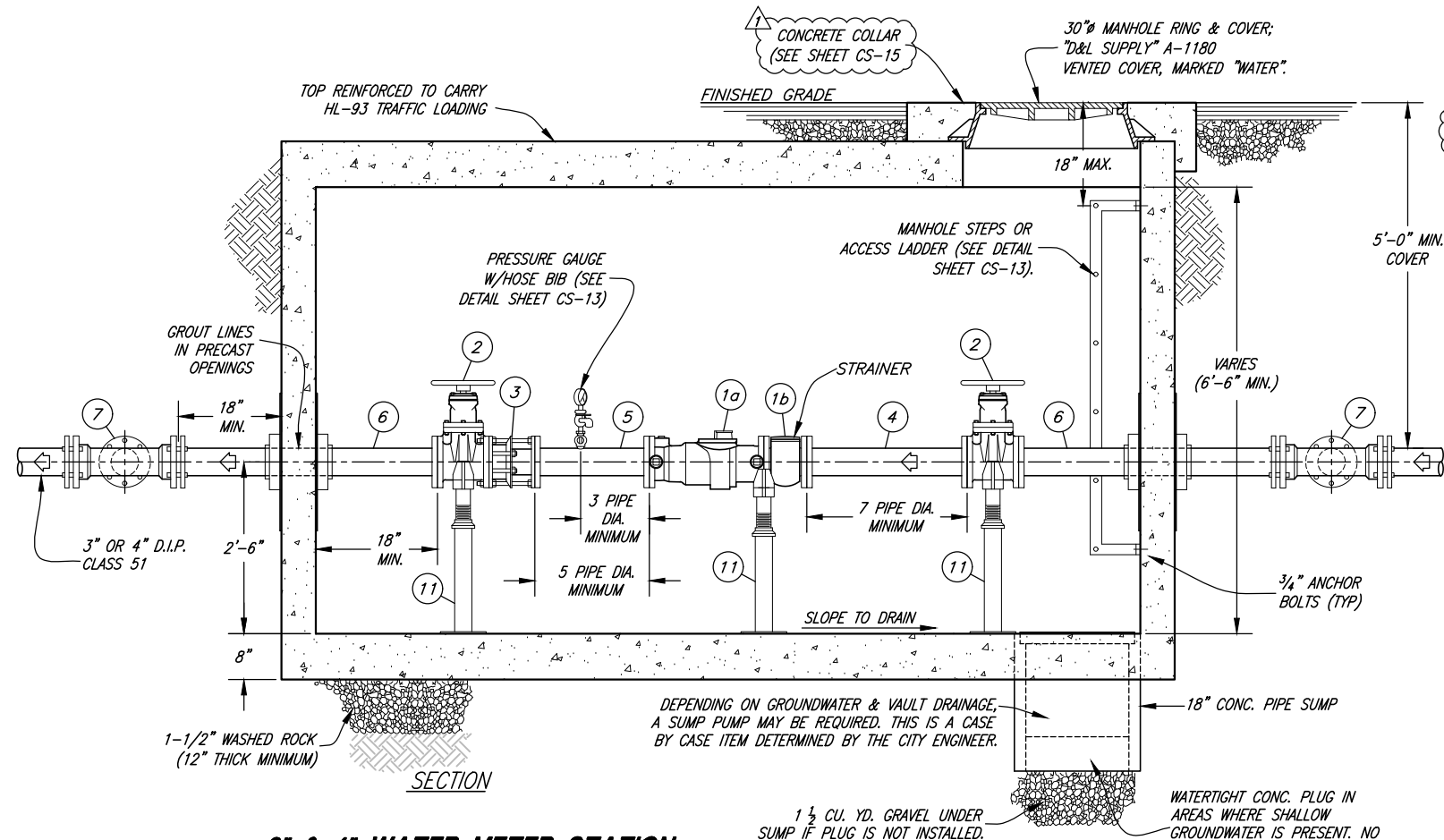
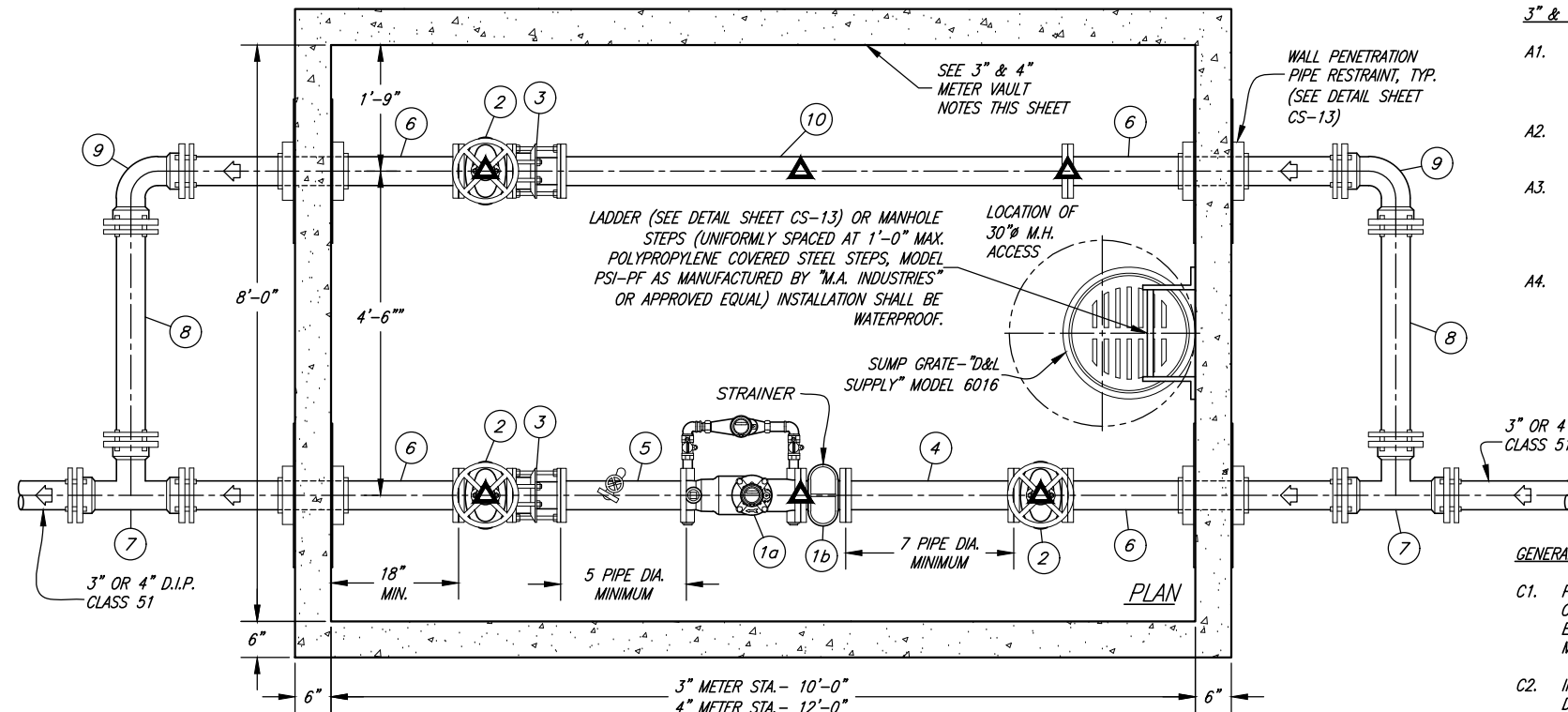
PUBLIC WORKS STANDARDS

CULINARY WATER - TRACER WIRE  
INSTALLATION DETAILS

SHEET:

CS-10

OF 24 SHEETS  
0



**3" & 4" WATER METER STATION**

**3" & 4" METER VAULT NOTES:**

- A1. ALL FITTINGS OUTSIDE OF THE VAULT ARE TO BE DUCTILE IRON MJ WITH THRUST RESTRAINT RETAINER GLANDS ("ROMAC", MJRG, OR APPROVED EQUAL).
- A2. PENETRATION WALLS NEED TO BE ADEQUATELY DESIGNED STRUCTURALLY FOR ANTICIPATED THRUST.
- A3. THE PRECAST VAULT MANUFACTURER IS RESPONSIBLE FOR DESIGN RELATED TO TRAFFIC LOADING AND THRUST. VERIFICATION OF PROPER DESIGN MUST BE PROVIDED TO THE CITY BY THE DEVELOPER, CONTRACTOR, OR PROPERTY OWNER AS THE CASE MAY BE.
- A4. ALL FITTINGS SHALL BE AWWA C-110 WITH 125 LB. FLANGES. ALL PIPING SHALL BE DUCTILE IRON PIPE CLASS 350 P.S.I. MIN.

NOTE:  
ALL SPECIFIED BRANDS OF MATERIALS SHOWN ON THESE DRAWINGS ARE "CITY STANDARDS." OTHER EQUIVALENT BRANDS MAY BE USED WITH THE PRIOR APPROVAL OF THE CITY ENGINEER AND THE CITY WATER SYSTEM SUPERINTENDENT.

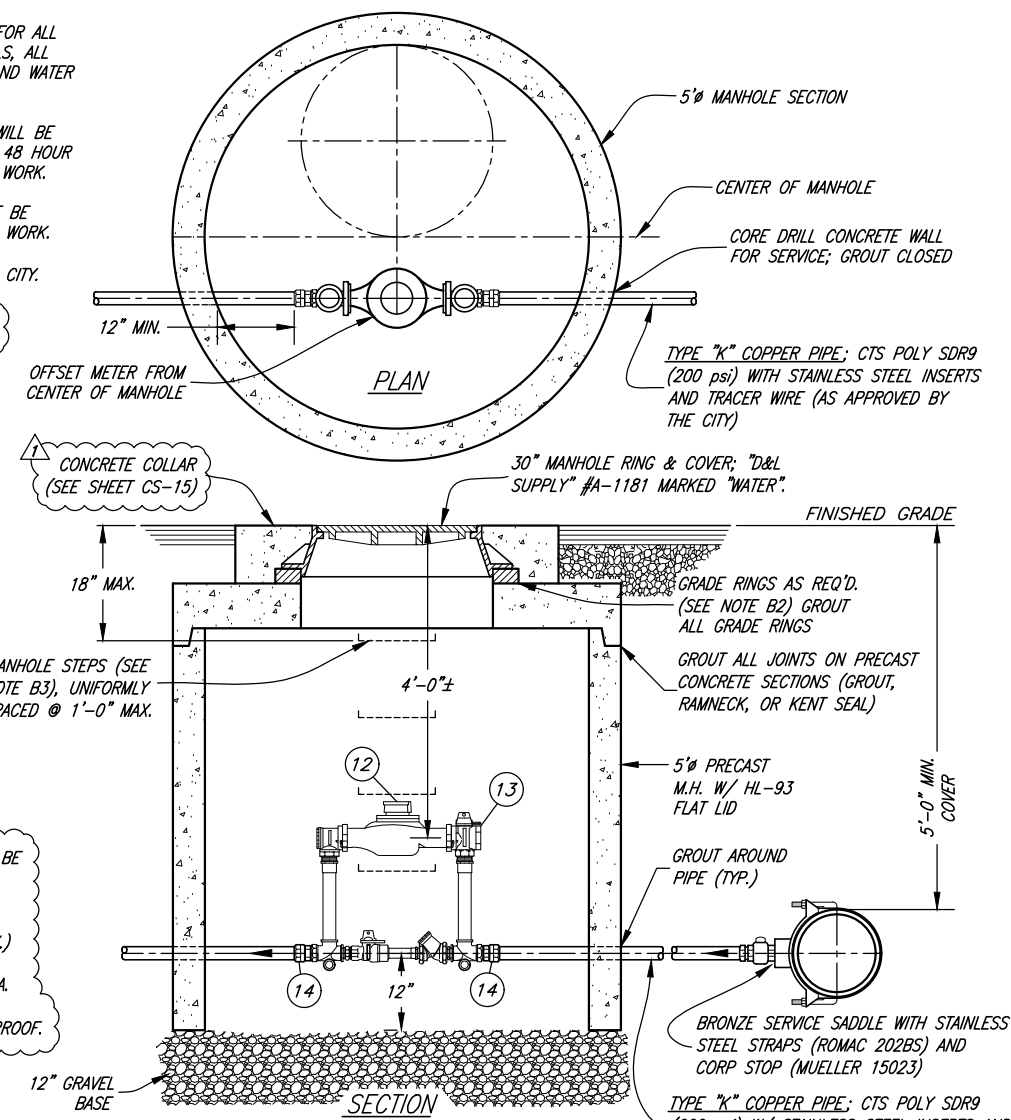
**GENERAL NOTES:**

- C1. PROPERTY OWNER OR CONTRACTOR SHALL PAY FOR ALL COSTS OF INSTALLATION INCLUDING ALL MATERIALS, ALL EXCAVATION AND FILL, ASPHALT REPLACEMENT AND WATER MAIN CONNECTION.
- C2. INSPECTION OF ALL WATER LINE INSTALLATIONS WILL BE DONE BY THE CITY WATER DEPARTMENT, WITH A 48 HOUR MINIMUM NOTICE REQUIRED PRIOR TO START OF WORK.
- C3. IF APPLICABLE, A CITY EXCAVATION PERMIT MUST BE REQUESTED AND APPROVED PRIOR TO START OF WORK.
- C4. "BLUE" BOLTS AND NUTS ARE REQUIRED BY THE CITY.
- C5. CONTRACTOR TO SUPPLY ALL METERS 1 1/2" OR LARGER.

**PIPE & FITTING SCHEDULE**

NO.	DESCRIPTION (3" & 4" METER STA.)	JOINT TYPE	3" LINE	4" LINE
1a	"MASTER METER" DUAL BODY COMPOUND (DBC) METER W/ STRAINER & 3G INTEGRATED REGISTER	FL	3"	4"
2	"MUELLER" RESILIENT WEDGE GATE VALVE W/ HANDWHEEL	FL	3" A-2362	4" A-2361
3	"ROMAC" DJ400 DISMANTLING JOINT	FL	3"	4"
4	SPOOL PIECE (7 PIPE DIA. MINIMUM)	FL	3"	4"
5	SPOOL PIECE (5 PIPE DIA. MINIMUM)	FL	3"	4"
6	NIPPLE	FLxPE	3"	4"
7	TEE	MJ	3"	4"
8	PIPE SECTION	PE	3"	4"
9	90° ELBOW	MJ	3"	4"
10	SPOOL PIECE	FL	3"	4"
11	"CLOW" F-1608 OR "ANVIL" #264 GALV. PIPE SUPPORT W/ COMPANION FLANGE & VARIABLE HEIGHT NIPPLE (6 EA REQ'D)			

NO.	DESCRIPTION (1 1/2" & 2" METER STA.)	JOINT TYPE	1 1/2" LINE	2" LINE
12	"MASTER METER" INTERMEDIATE MULTI-JET METER W/3G INTEGRATED REGISTER	FL	1 1/2"	2"
13	"MUELLER" B-2423-2 METER YOKE (18" RISER)	-	1 1/2"	2"
14	"MUELLER" 110 COMPRESSION CONN. COUPLING	-	1 1/2"	2"



**1 1/2" & 2" WATER METER STATION**

**1 1/2" & 2" METER NOTES:**

- B1. 1 1/2" SERVICE LINE-13" METER  
2" SERVICE LINE-17" METER
- B2. NO MORE THAN 12" OF GRADE RINGS TO BE ALLOWED ON ANY MANHOLE
- B3. MANHOLE STEPS (FOR MANHOLES OVER 6' DEEP.) UNIFORMLY SPACED (1'-0" MAX.) POLYPROPYLENE COVERED STEEL STEPS, MODEL PSI-PF AS MANUFACTURED BY "M.A. INDUSTRIES" OR APPROVED EQUAL - INSTALLATION OF STEPS SHALL BE WATERPROOF.



PROJECT ENGINEER  
2-12-2019  
DATE

REV.	DATE	APPR.	MODIFIED NOTES
1	JAN '19	BKJ	MODIFIED NOTES

SCALE:  
N.T.S.

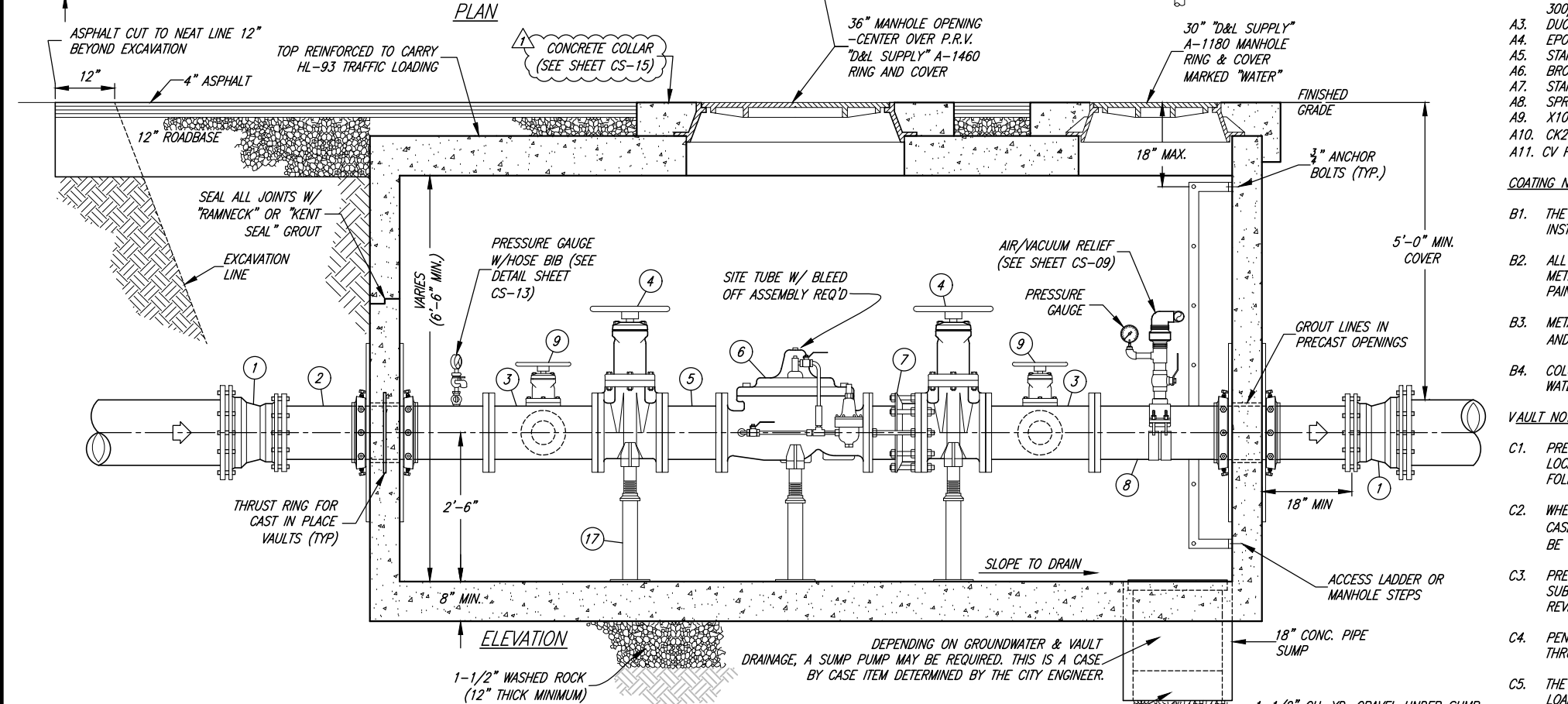
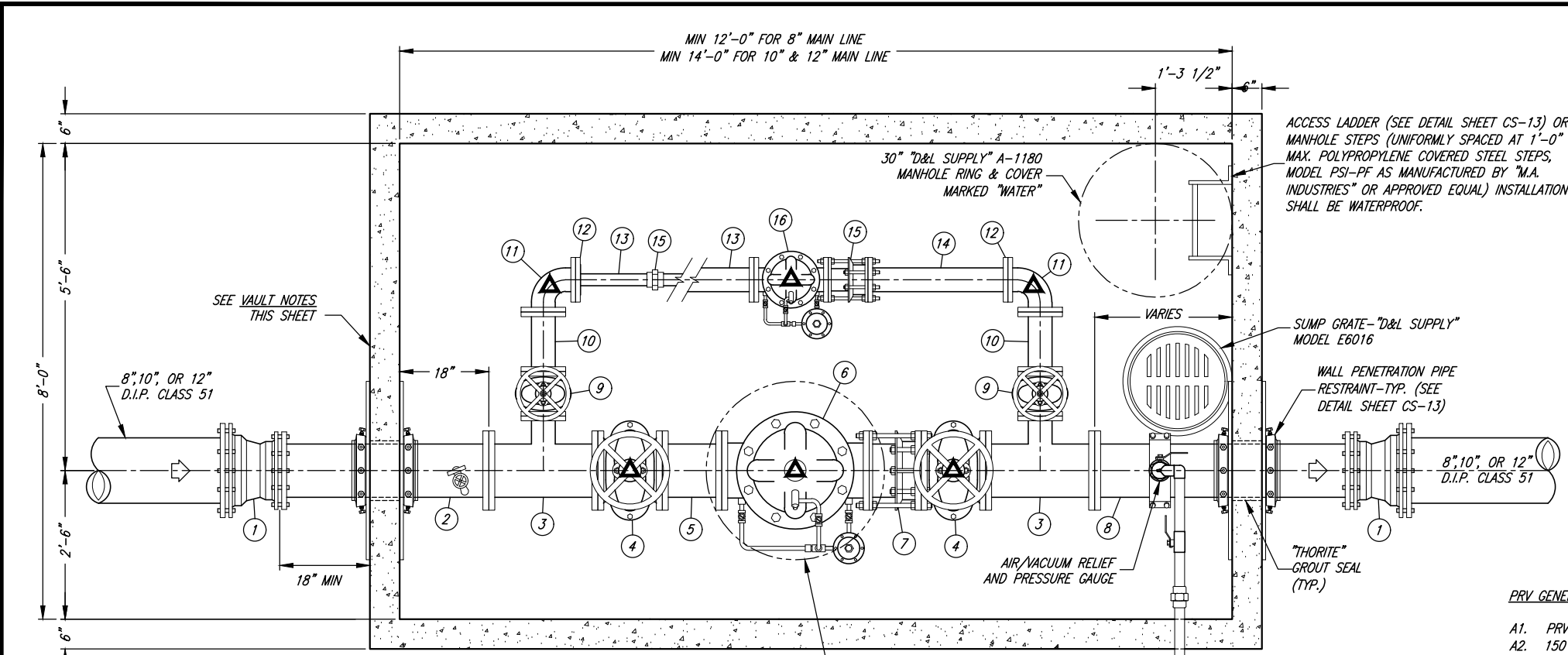
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**PUBLIC WORKS STANDARDS**  
**CULINARY WATER - STANDARD WATER METER STATIONS**

SHEET:  
**CS-11**  
OF 24 SHEETS  
0



**PRESSURE REDUCTION STATION**

PIPE & FITTING SCHEDULE					
NO.	DESCRIPTION	JOINT TYPE	8" LINE	10" LINE	12" LINE
1	D.I. REDUCER (2)	MxMJ	8"x6"	10"x8"	12"x10"
2	D.I. NIPPLE PIECE	FLxPE	6"	8"	10"
3	D.I. REDUCING TEE (2)	FL	6"x6"x4"	8"x8"x4"	10"x10"x4"
4	"MUELLER" A-2361 GATE VALVE W/ HANDWHEEL (2)	FL	6"	8"	10"
5	12" D.I. SPOOL PIECE	FL	6"	8"	10"
6	"CLA-VAL" 90-01 PRESSURE REDUCTION VALVE	FL	6"	8"	10"
7	"ROMAC" DJ400 DISMANTLING JOINT	FL	6"	8"	10"
8	D.I. NIPPLE PIECE	FLxPE	6"	8"	10"
9	"MUELLER" A-2361 GATE VALVE W/ HANDWHEEL (2)	FL	4"	4"	4"
10	12" D.I. SPOOL PIECE	FL	4"	4"	4"
11	D.I. 90° ELBOW (2)	FL	4"	4"	4"
12	BLIND FLANGE W/ THR. CONNECTION (2)	FLxTHR.	4"x2"	--	--
13	D.I. SPOOL PIECE BRASS PIPE	THR.	2"	--	--
14	D.I. SPOOL PIECE BRASS PIPE	THR.	2"	--	--
15	"ROMAC" DJ400 DISMANTLING JOINT BRASS UNION	THR.	2"	--	--
16	"CLA-VAL" 90-01 PRESSURE REDUCTION VALVE	THR.	2"	--	--
17	"CLOW" F-1608 OR "ANVIL" #264 GALV. PIPE SUPPORT W/ 3" COMPANION FLANGE & VARIABLE HEIGHT 3" NIPPLE (6 EA REQ'D.)				

**PRV GENERAL SPECIFICATIONS:**

- A1. PRV TO BE CLA-VAL #90-01 YBCSKC
- A2. 150 # FLANGED FOR 250 PSI WORKING PRESSURE, 300# FLANGED IF GREATER THAN 250 PSI
- A3. DUCTILE IRON BODY GLOBE PATTERN
- A4. EPOXY LINED AND COATED
- A5. STAINLESS STEEL INTERNAL TRIM
- A6. BRONZE PILOT CONTROLS
- A7. STAINLESS STEEL TUBES & FITTINGS
- A8. SPRING RANGES FOR PRESSURE REDUCING PILOT
- A9. X101 VALVE POSITION INDICATOR
- A10. CK2 ISOLATION BALL VALVES (STAINLESS)
- A11. CV FLOW CONTROL (OPENING)

**COATING NOTES:**

- B1. THE P.R.V. VALVE SHALL INCLUDE FACTORY INSTALLED INTERIOR EPOXY COATING.
- B2. ALL NEW AND EXISTING PIPING, VALVES, FITTINGS, METERS, ETC, INSIDE THE VAULT SHALL BE EPOXY PAINTED.
- B3. METAL SURFACES TO BE PAINTED SHALL BE PRIMED AND THEN PAINTED W/ TWO COATS OF EPOXY PAINT.
- B4. COLORS AS DIRECTED BY THE CITY ENGINEER OR CITY WATER SYSTEM SUPERINTENDENT.

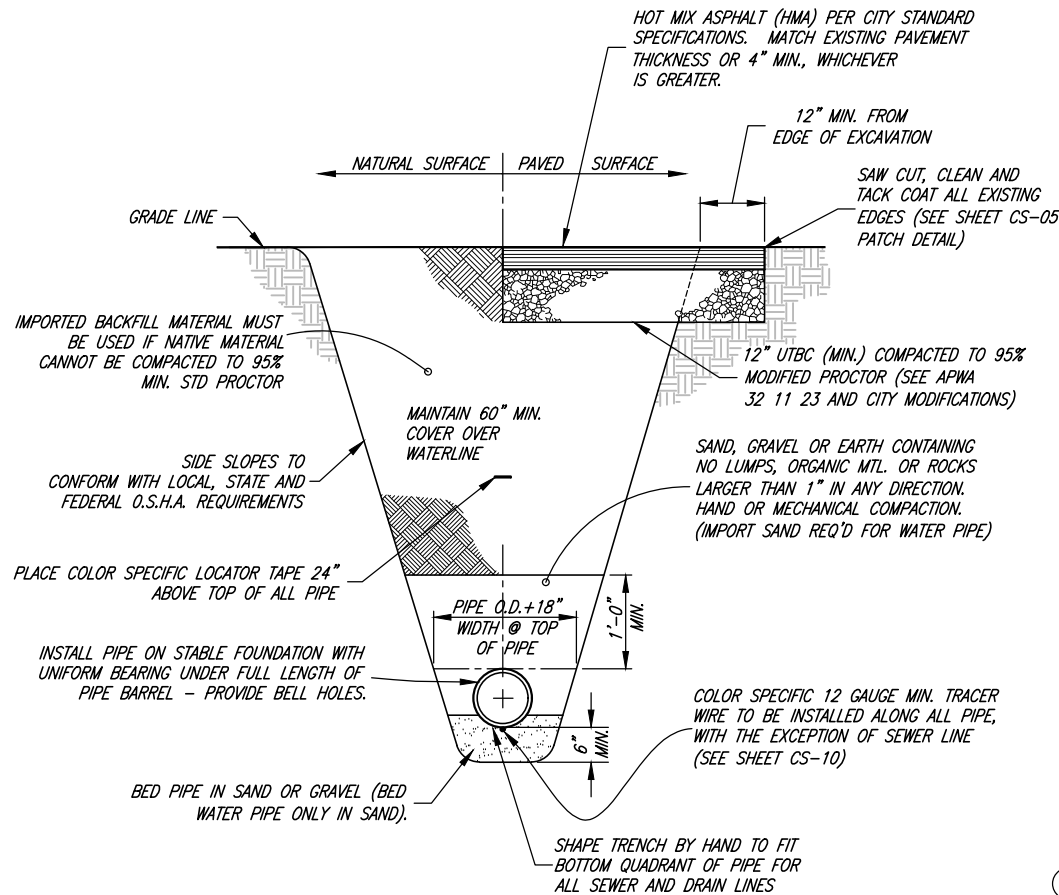
**VAULT NOTES:**

- C1. PRE-PLUMBED PRV VAULTS ARE THE PREFERRED OPTION FOR INSTALLATION. THE USE AND LOCATION OF A PRE-PLUMBED PRV VAULT SHALL BE AS DIRECTED BY THE CITY ENGINEER FOLLOWING REVIEW OF CURRENT SITE CONDITIONS.
- C2. WHERE APPLICABLE, PRESSURE RELIEF VALVE ASSEMBLY MAY BE REQUIRED. THIS IS A CASE BY CASE ITEM DETERMINED BY THE CITY WATER DEPARTMENT (PRV VAULT WILL NEED TO BE LENGTHENED TO ACCOMMODATE SUCH VALVE)
- C3. PRECAST CONCRETE STRUCTURE CAN BE REPLACED WITH CAST-IN-PLACE CONCRETE VAULT. SUBMIT ENGINEERED CONSTRUCTION PLANS WITH REBAR DETAILS TO CITY ENGINEER FOR REVIEW AND ACCEPTANCE PRIOR TO CONSTRUCTION.
- C4. PENETRATION WALLS NEED TO BE ADEQUATELY DESIGNED STRUCTURALLY FOR ANTICIPATED THRUST.
- C5. THE PRECAST VAULT MANUFACTURER IS RESPONSIBLE FOR DESIGN RELATED TO HL-93 TRAFFIC LOADING AND THRUST. VERIFICATION OF PROPER DESIGN MUST BE PROVIDED TO THE CITY BY THE DEVELOPER, CONTRACTOR, OR PROPERTY OWNER AS THE CASE MAY BE.

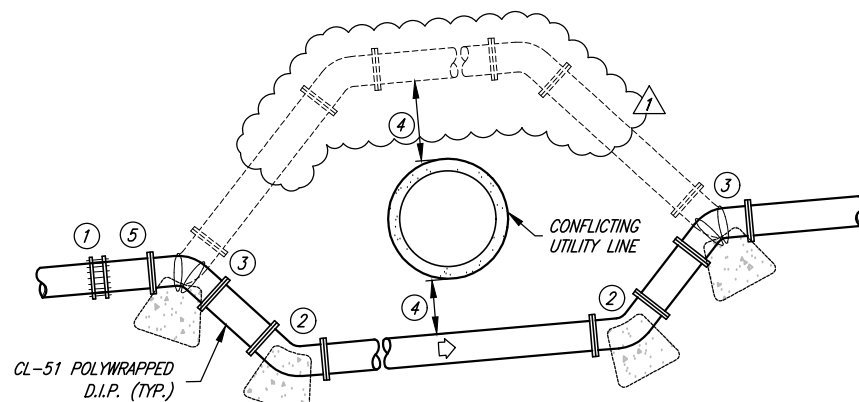
**GENERAL NOTES:**

- A. "BLUE" BOLTS AND NUTS ARE REQUIRED BY THE CITY.
- B. ALL FITTINGS OUTSIDE OF THE VAULT ARE TO BE DUCTILE IRON MJ WITH THRUST RESTRAINT RETAINER GLANDS ("ROMAC", MJRG, OR APPROVED EQUAL)
- C. STRUCTURE, PIPING & VALVE SIZES FOR P.R.V. STATIONS ON LINE SIZES GREATER THAN 12" SHALL BE SPECIFIED BY THE CITY ENGINEER.
- D. ALL SPECIFIED BRANDS OF MATERIALS SHOWN ON THESE DRAWINGS ARE "CITY STANDARDS." OTHER EQUIVALENT BRANDS MAY BE USED WITH THE PRIOR APPROVAL OF THE CITY ENGINEER AND THE CITY WATER SYSTEM SUPERINTENDENT.



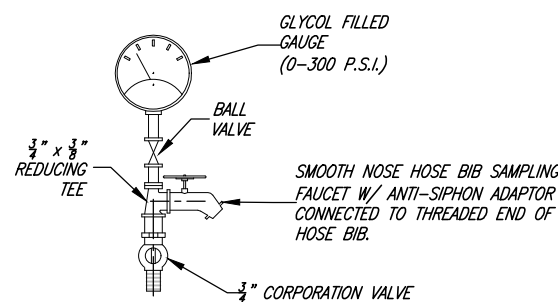


**TYPICAL TRENCH SECTION**  
(WATER, IRRIGATION, SEWER, STORM DRAIN, AND LAND DRAIN)



**TYPICAL WATERLINE LOOP**

- ① TRANSITION COUPLING; "ROMAC" MODEL 501
- ② MJ 45° BEND W/RETAINER GLANDS
- ③ CONSTRUCT THRUST BLOCKS AT EACH 45° BEND W/(3) #6 REBAR SECURING BLOCK TO FITTING (EPOXY COATING)
- ④ MINIMUM OF 12" COVER BETWEEN THE WATERLINE AND CONFLICTING UTILITY LINE TO BE CROSSED, EXCEPT LOOPS INVOLVING SEWER MAINS WHERE A MINIMUM OF 18" VERTICAL COVER ABOVE THE SEWER MAIN IS REQUIRED. EXCEPTIONS MUST BE APPROVED BY THE UTAH DIVISION OF DRINKING WATER (DDW.)
- ⑤ AN AIR/VACUUM RELIEF VALVE MAY BE REQUIRED ON A CASE BY CASE BASIS AS DIRECTED BY THE CITY WATER SYSTEM SUPERINTENDENT.



**PRESSURE GAUGE**  
W/SAMPLING FAUCET DETAIL

**TRENCH NOTES:**

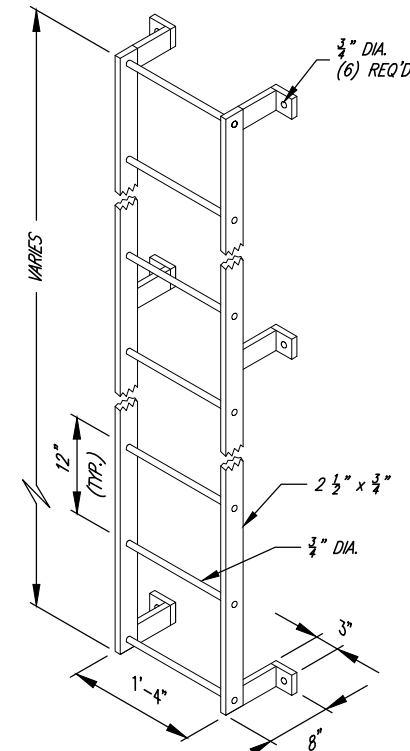
- A. BACKFILL PER APWA 33 05 20 AND CITY MODIFICATIONS.
- B. COMPACTION TEST REQUIRED AT SPRING-LINE FOR ALL P.V.C. OR H.D.P.E. PIPES.
- C. PAVEMENT RESTORATION PER APWA 33 05 25 AND CITY MODIFICATIONS.
- D. GRAVEL SURFACED AREAS, SUCH AS ROADS AND SHOULDERS, PARKING AREAS, AND UNPAVED DRIVEWAYS, SHALL BE REPAIRED WITH 8" THICK (MIN.) 1" UNTREATED BASE COURSE COMPACTED TO 95% MODIFIED PROCTOR.
- E. WATER & SEWER LINES, INCLUDING SERVICE LINES, SHALL NOT BE INSTALLED IN THE SAME TRENCH.

**PIPE RESTRAINT**

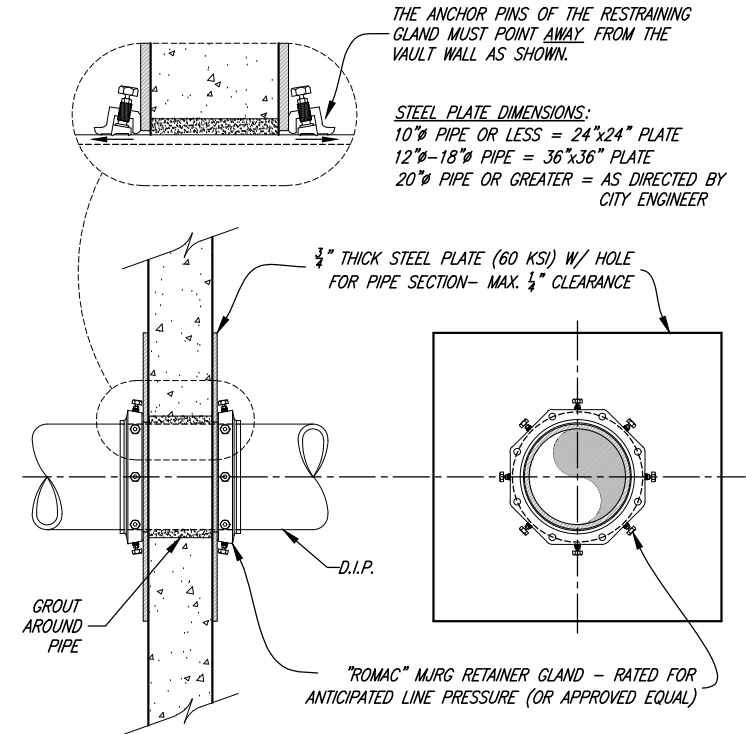
- A1. FOR NOMINAL PIPE DIAMETERS 8" AND GREATER, ALL BENDS, CROSSES, TEES, REDUCERS, AND VALVES SHALL BE INSTALLED WITH RESTRAINING JOINTS ("MEGA-LUG" OR APPROVED EQUAL).
- A2. DESIGN SHALL ALSO BE REQUIRED TO ENSURE ADEQUATE RESTRAINT FOR PIPING JOINTS NEAR FITTINGS BASED ON PIPE DIAMETER AND PIPE PRESSURE.

**THRUST BLOCKING NOTES:**

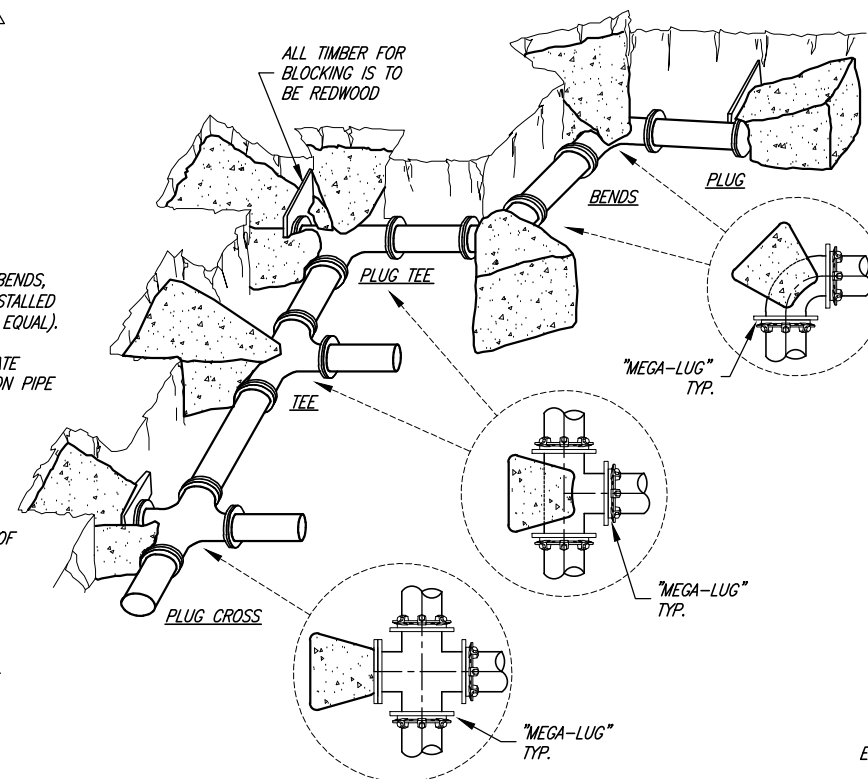
- B1. CONCRETE SHALL NOT BE PLACED WITHIN 1-1/2" OF JOINTS AND BOLTS. COVER ALL METAL CONTACT AREAS WITH A POLY WRAP PRIOR TO CONCRETE PLACEMENT.
- B2. IN THE ABSENCE OF A SOILS REPORT, ALL THRUST BLOCKS SHALL BE SIZED ON THE BASIS OF A MAXIMUM LATERAL BEARING VALUE FOR 2000 P.S.F. AND A THRUST RESULTING FROM 200% OF THE WATER LINE STATIC LINE TEST.
- B3. THRUST BLOCKS ARE REQUIRED AT ALL BENDS OF 22-1/2" OR MORE. 11-1/4" BENDS SHALL HAVE RETAINER GLANDS.
- B4. CONCRETE SHALL HAVE A MINIMUM COMPRESSIVE STRENGTH OF 3000 PSI IN 28 DAYS.



**LADDER DETAIL**  
HOT DIP GALVANIZE AFTER FABRICATION



**WALL PENETRATION DETAIL**  
FOR PRECAST VAULT (TYP.)



**TYPICAL RETAINER GLANDS & THRUST BLOCKING**

THRUST PER PSI OF WATER PRESSURE AT VARIOUS FITTINGS				
PIPE SIZE (IN.)	DEAD END OR TEE (LB.)	90° ELBOW (LB.)	45° ELBOW (LB.)	22-1/2° ELBOW (LB.)
4	19	27	15	7
6	39	55	30	15
8	67	94	51	26
10	109	154	84	43
12	155	218	119	61
14	210	296	161	82
16	272	383	209	106
18	351	494	269	137
20	434	611	333	169
24	623	878	487	244
30	947	1,332	722	377
36	1,356	1,905	1,032	542

**NOTES:**

- C1. IN USING THE ABOVE TABLE, USE THE MAXIMUM INTERNAL PRESSURE ANTICIPATED (I.E. HYDROSTATIC TEST PRESSURE, POSSIBLE SURGE PRESSURE DUE TO PUMP SHUT OFF, ETC.).
- C2. SEE SOILS REPORT FOR BEARING STRENGTH OF SOIL. IN THE ABSENCE OF A SOILS REPORT, AN AVERAGE SOIL (SPADABLE MEDIUM CLAY) CAN BE ASSUMED TO HAVE A BEARING STRENGTH OF 2000 P.S.F.

**EXAMPLE:**

8-INCH 90° ELBOW, PRESSURE 200 LB./SQ. IN.  
FROM TABLE: THRUST = 94 X 200 = 18,800 LB.  
ASSUME BEARING STRENGTH = 2,000 LB./SQ. FT.

18,800 / 2,000 = 9.4 SQ. FT. AREA OF BEARING REQUIRED FOR THRUST BLOCK



BRANDON KENT JONES  
No. 5148758  
PROJECT ENGINEER  
2-12-2019  
DATE

REV.	DATE	APPR.
1	JAN '19	BKJ

SCALE:  
N.T.S.

DESIGNED BKJ  
DRAWN BEB  
CHECKED BKJ

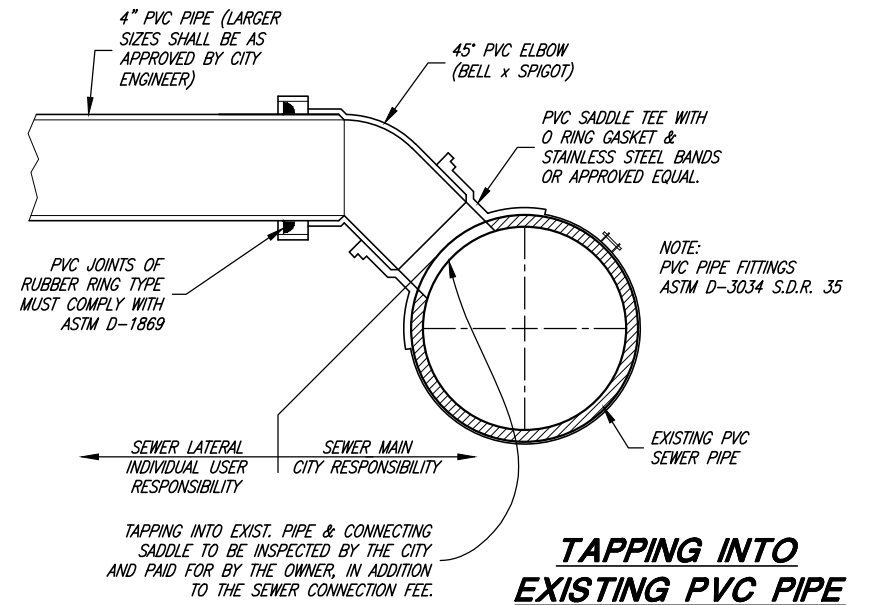


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**SOUTH WEBER CITY CORPORATION**  
**PUBLIC WORKS STANDARDS**  
**CULINARY WATER - THRUST BLOCK, WATERLINE LOOP, PIPE TRENCH & MISC. VAULT DETAILS**

SHEET:  
**CS-13**  
OF 24 SHEETS  
0

1. ALL SEWER MAIN LATERAL CONNECTIONS ON SEWER MAINS IN NEW SUBDIVISIONS SHALL BE MADE WITH IN LINE PRE-FORMED WYES OR TEES UNLESS OTHERWISE APPROVED BY THE CITY ENGINEER.
2. FLOWLINE ELEVATION OF LATERALS SHALL EQUAL THE INSIDE TOP OF PIPE ON MAINLINE AT THE CONNECTING POINT (THE LATERAL TAP SHALL BE IN THE TOP QUARTER OF THE SEWER MAIN LINE PREFERABLY IN THE 10:00 OR 2:00 POSITION).
3. SANITARY SEWER SERVICE LATERAL CONNECTIONS SHALL NOT BE ALLOWED IN SEWER MANHOLES.
4. SANITARY SEWER MAINS AND LATERALS SHALL BE "GREEN" IN COLOR. LAND DRAIN MAIN LINES SHALL BE "GREEN" IN COLOR AND LAND DRAIN LATERAL LINES SHALL BE "WHITE" IN COLOR. IRRIGATION PIPES SHALL BE "PURPLE" IN COLOR. PREVIOUS YEARS PIPE COLORS VARY THROUGHOUT THE CITY. CONTRACTOR TO VERIFY EXISTING PIPE PRIOR TO MAKING ANY CONNECTION,
5. INSERTA TEE PRODUCT IS NOT APPROVED BY THE CITY
6. ALL CLEANOUTS SHALL BE MARKED AND FITTED WITH A METAL LID FOR LOCATION PURPOSES
7. ALL CULINARY WATER MAINS AND SERVICES MUST MAINTAIN A MINIMUM SEPARATION ABOVE ALL SEWER MAINS AND LATERALS OF 18" VERTICAL AND 10'-0" HORIZONTAL IN ACCORDANCE WITH THE STATE OF UTAH DIVISION OF DRINKING WATER (DDW) RULES SECTION R309-550-7. EXCEPTIONS MUST BE APPROVED BY DDW.
8. ALL SANITARY SEWER LINES SHALL BE INSPECTED BY MEANS OF VIDEO CAMERA WHEN CONSTRUCTED.
9. STAMPED STAINLESS STEEL PINS USED FOR LATERAL LOCATING ARE REQUIRED BY THE CITY. BLANK S.S. PINS SHALL BE PROVIDED BY THE CITY AND INSTALLED AND STAMPED BY THE CONTRACTOR DURING ALL NEW CONSTRUCTION OR RESTORED WHEN REPLACING DAMAGED CURB & GUTTER DUE TO ANY CONSTRUCTION RELATED ACTIVITY. S.S. PINS SHALL BE STAMPED "S" FOR SANITARY SEWER, "W" FOR CULINARY WATER, AND "L" FOR LAND DRAIN.
10. DOWNSTREAM LAND DRAIN CONNECTION TO AN EXISTING STORM DRAIN SYSTEM IS REQUIRED.



Brandon K. Jones  
PROJECT ENGINEER  
2-12-2019  
DATE

1	JAN '19	BKJ	DELETED EX. DETAIL; MODIFIED NOTES
REV	DATE	ADD	

SCALE:

N.T.S.

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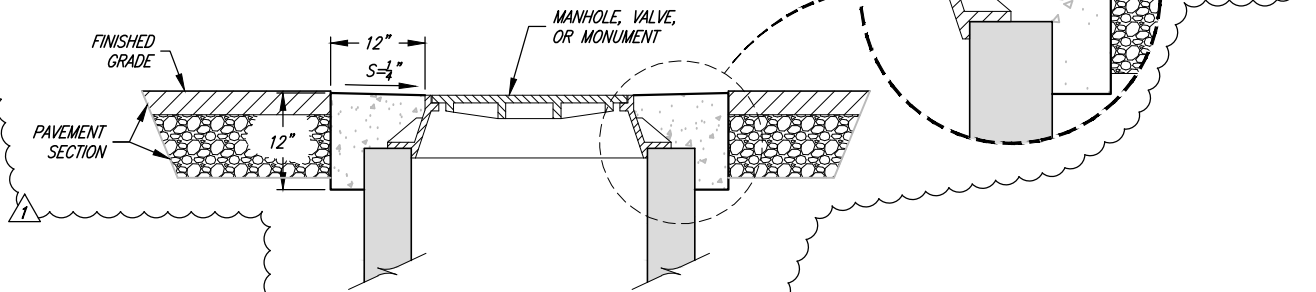
***SOUTH WEBER CITY CORPORATION***  
***PUBLIC WORKS STANDARDS***

## SANITARY SEWER - LATERAL & CONNECTION DETAILS

SHEET:  
**-14**  
SHEETS

# CONCRETE COLLAR NOTES:

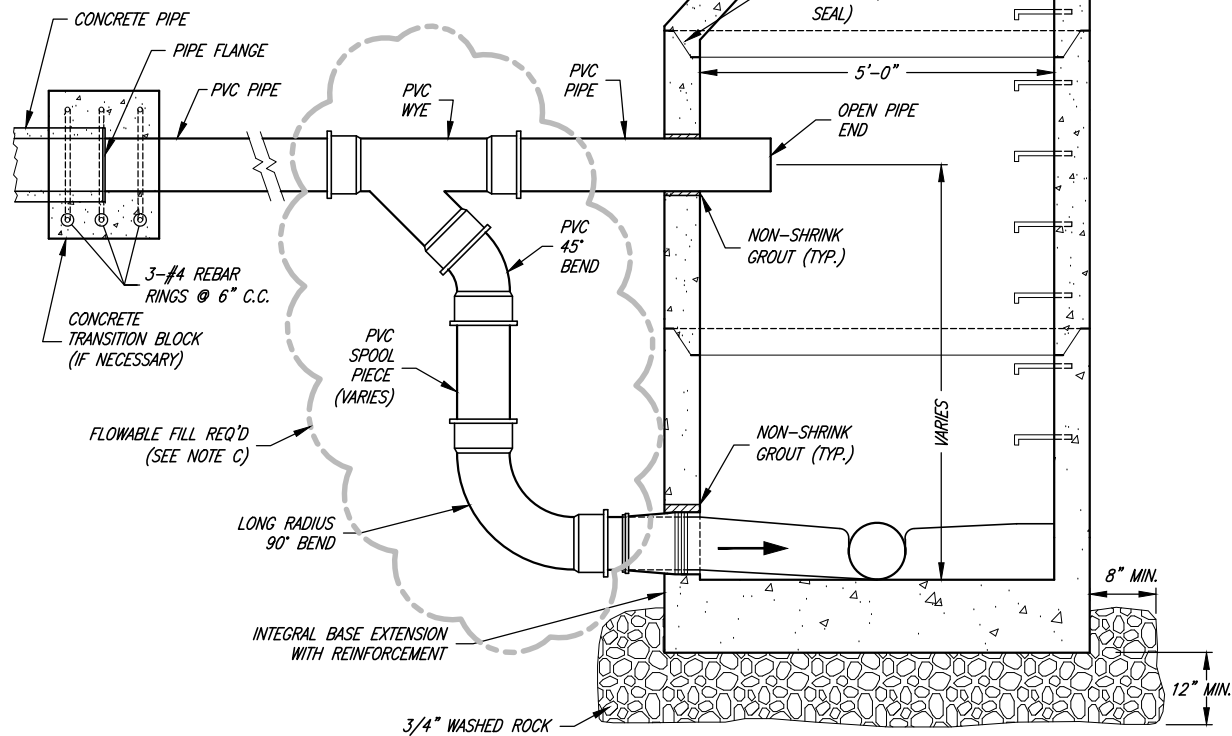
- A1. ALL CONCRETE COLLARS TO BE INSTALLED WITHIN 14 DAYS AFTER PAVING.
- B1. COLLARS AROUND MANHOLES AND CULINARY WATER VALVES ARE TO BE ROUND.
- C1. COLLARS AROUND IRRIGATION VALVES ARE TO BE SQUARE.
- D1. FIBER MESH SHALL BE ADDED TO ALL CONCRETE.



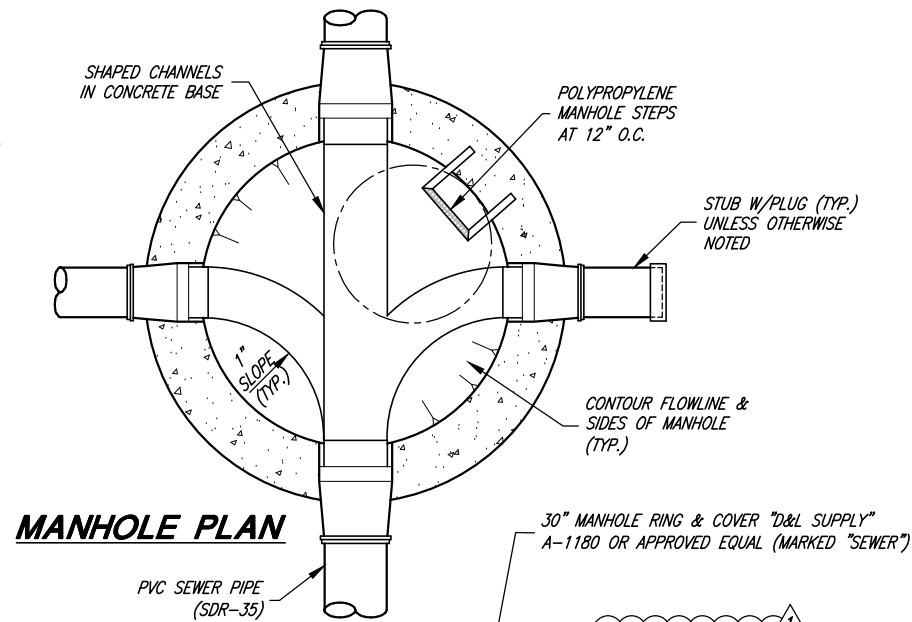
CONCRETE COLLAR DETAIL

# DROP MANHOLE NOTES:

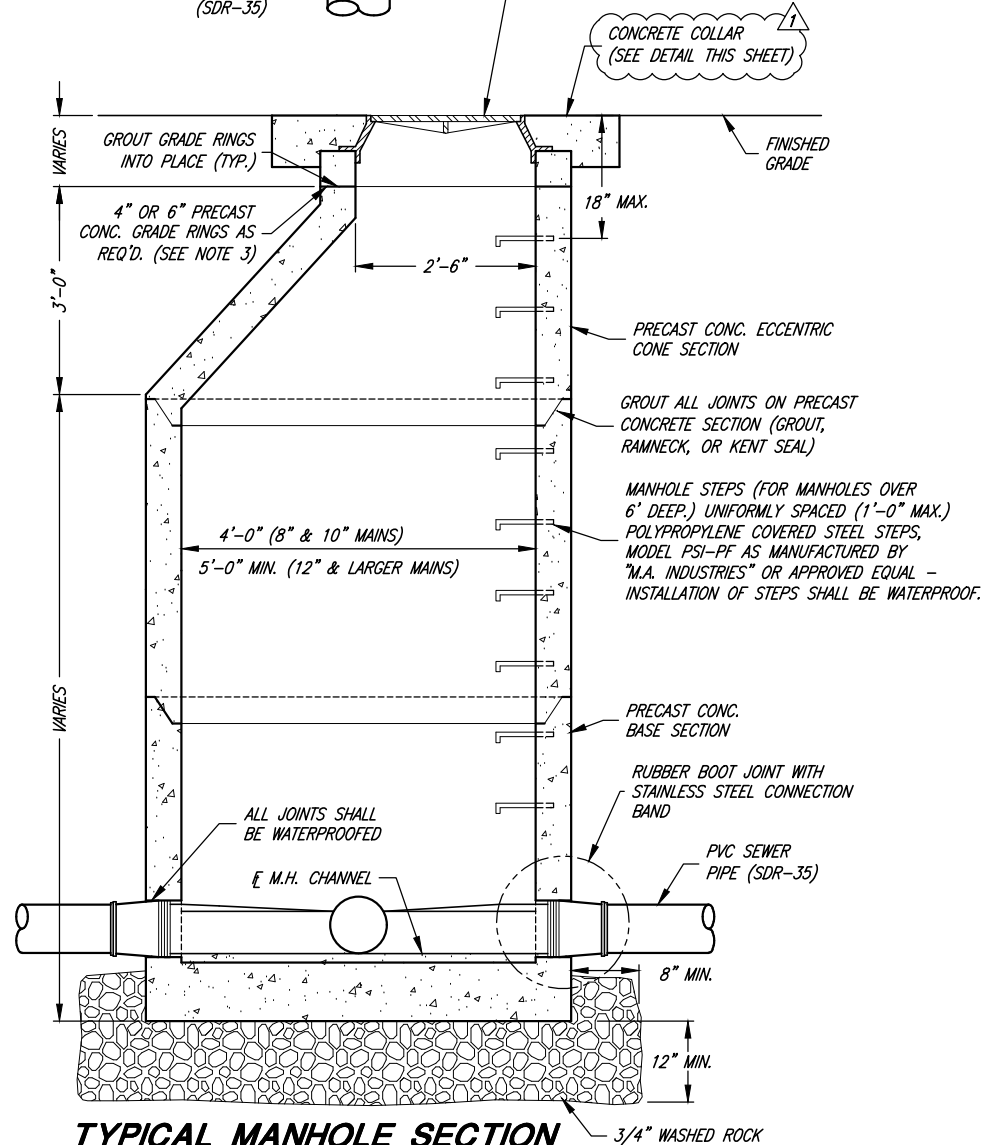
- A. USE DROP MANHOLE ONLY WHEN DROP EXCEEDS 2'-0" (UAC R317-3)
- B. DROP MANHOLE SHALL CONSIST OF ASTM D3034 SDR 35 PVC PIPE WITH SDR 35 PVC GASKETED FITTINGS.
- C. DUE TO THE UNEQUAL EARTH PRESSURES THAT WOULD RESULT FROM THE BACKFILLING OPERATION IN THE VICINITY OF THE MANHOLE, THE ENTIRE OUTSIDE DROP CONNECTIONS SHALL BE ENCASED IN CONCRETE. (UAC R317-3)
- D. DROP MANHOLES SHOULD BE USED ON STREETS WITH GRADES 8% OR GREATER OR AS OTHERWISE DIRECTED BY THE CITY ENGINEER.



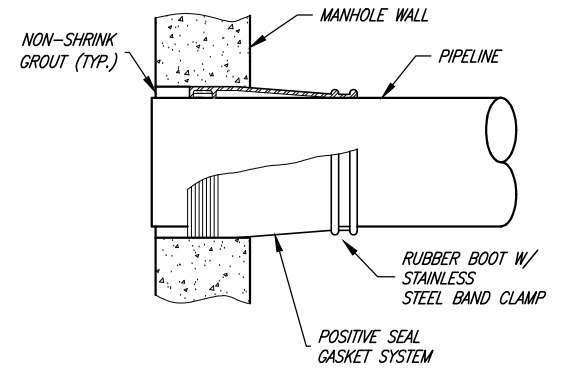
TYPICAL DROP MANHOLE SECTION



MANHOLE PLAN



TYPICAL MANHOLE SECTION



RUBBER BOOT DETAIL

# GENERAL NOTES:

1. SECURE INVERTS IN ALL MANHOLES DURING CONSTRUCTION SO AS TO PREVENT GRAVEL AND OTHER DEBRIS FROM COLLECTING INSIDE.
2. A LARGER DIAMETER MANHOLE MAY BE REQUIRED BY THE DESIGN ENGINEER AFTER EVALUATION OF THE NUMBER, SIZE, AND ANGLE OF THE PIPES THAT CONNECT TO THE MANHOLE.
3. NO MORE THAN 12" OF GRADE RINGS TO BE ALLOWED ON ANY MANHOLE.
4. ALL TERMINATING SEWER MAINS SHALL END WITH A CITY STANDARD MANHOLE.
5. SERVICE LATERAL CONNECTIONS SHALL NOT BE ALLOWED IN SEWER MANHOLES.
6. ALL SANITARY SEWER LINES SHALL BE INSPECTED BY MEANS OF VIDEO CAMERA AND AIR TESTED WHEN CONSTRUCTED. SEE APWA 33 08 00 AND CITY MODIFICATIONS FOR MORE INFORMATION.
7. WHERE THE DIFFERENCE IN ELEVATION BETWEEN THE INCOMING SEWER AND MANHOLE INVERT IS LESS THAN 24 INCHES, THE INVERT SHOULD BE FILLETED.
8. FLAT MANHOLE RINGS & COVERS (SLAB CONSTRUCTION) ARE NOT ALLOWED ON ANY MANHOLE CONE SECTION.



PROJECT ENGINEER  
2-12-2019  
DATE

REV.	DATE	APPR.	NOTES
1	JAN '19	BKJ	ADDED DETAIL; ADDED AND/OR MODIFIED NOTES

SCALE:  
N.T.S.

DESIGNED BKJ  
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CHECKED BKJ

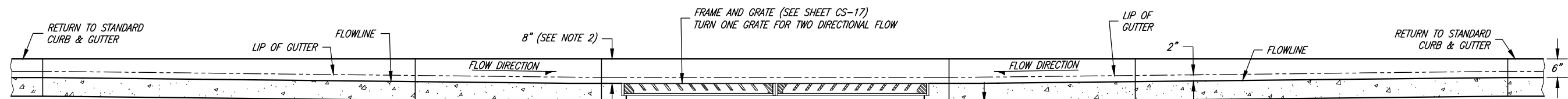
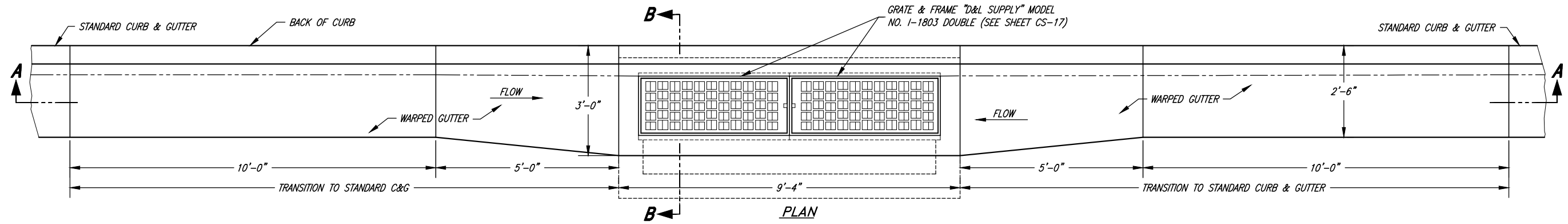


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SANITARY SEWER - TYPICAL MANHOLES & DETAILS

SHEET:  
CS-15  
OF 24 SHEETS  
0



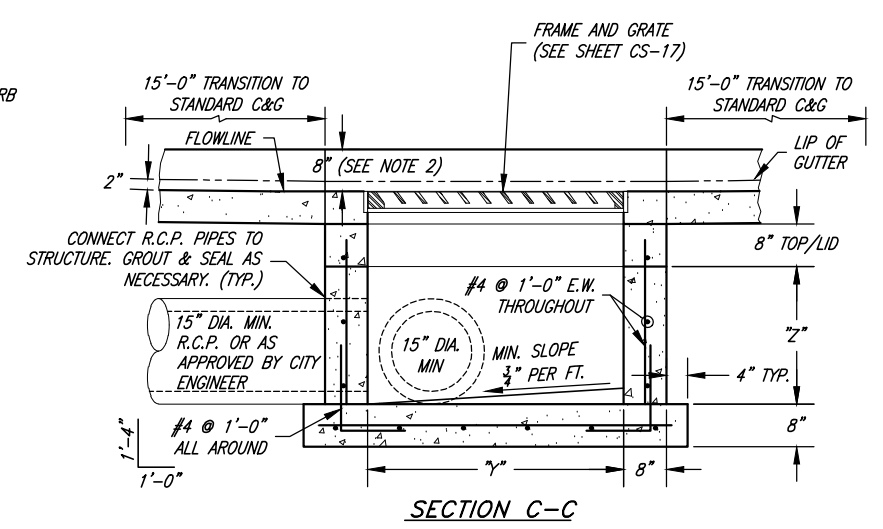
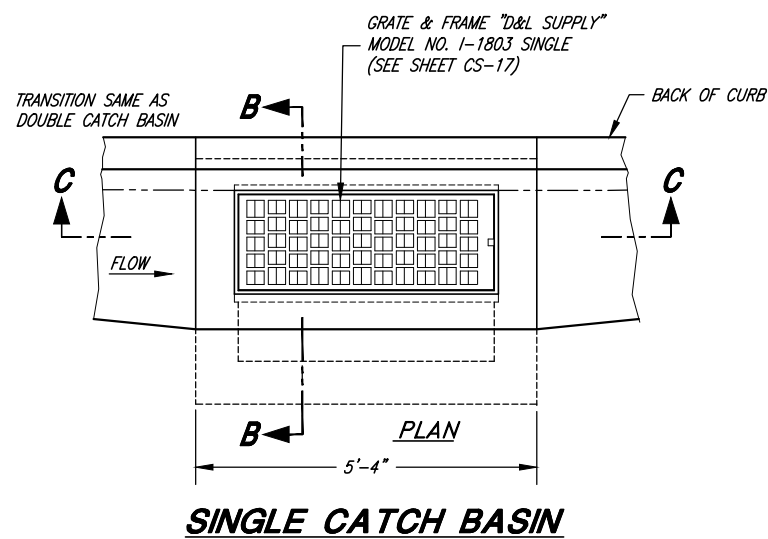
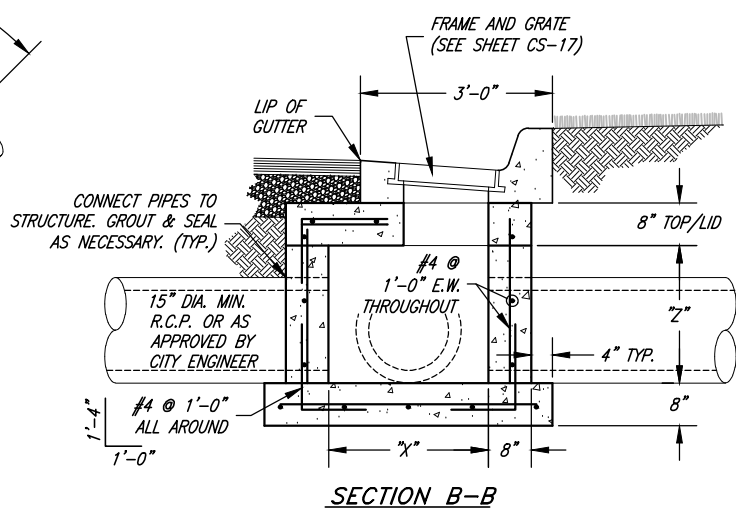
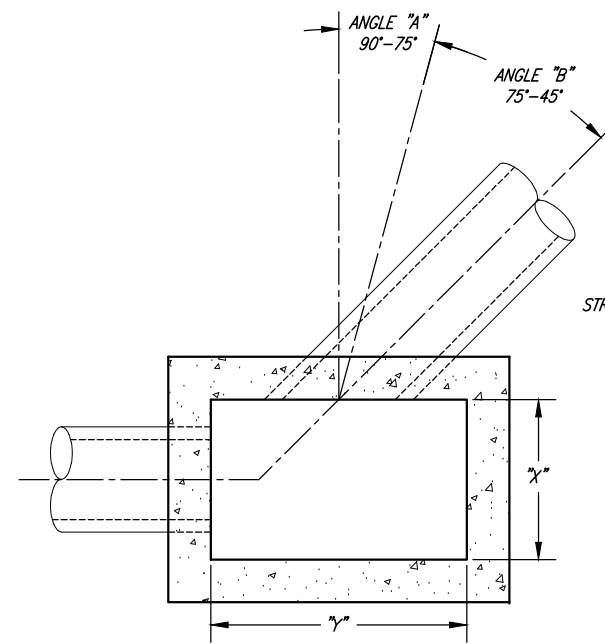


STANDARD CATCH BASIN DIMENSION TABLE					
PIPE SIZE (IN.)	"X"	SINGLE CATCH BASIN		DOUBLE	"Z" MIN.
		"Y" (ANGLE A)	"Y" (ANGLE B)	"Y"	
15	2'-6"	4'-0"	4'-0"	8'-0"	2'-0"
18	2'-6"	4'-0"	4'-0"	8'-0"	2'-6"
21	4'-0"	4'-0"	4'-0"	8'-0"	3'-0"
24	4'-0"	4'-0"	5'-0"	8'-0"	3'-0"
30	4'-0"	4'-0"	6'-0"	8'-0"	3'-6"
36	4'-0"	5'-0"	6'-0"	8'-0"	4'-0"
42	6'-0"	6'-0"	7'-0"	8'-0"	5'-0"
48	6'-0"	6'-0"	8'-0"	8'-0"	5'-6"

GENERAL NOTES:

- ALL CATCH BASIN BOX SIZES REFLECT DIMENSIONS FOR THE MINIMUM 15" PIPE SIZE. BOX DIMENSIONS MUST INCREASE PROPORTIONALLY TO ACCOMMODATE LARGER PIPE SIZES.
- DEPTH MAY VARY FROM 6" TO 10" AS DIRECTED BY THE CITY ENGINEER
- CAST-IN-PLACE CONCRETE CATCH BASINS CAN BE REPLACED WITH PRECAST CONCRETE CATCH BASINS WITH HL-93 DECK LOADING AND COMPARABLE SIZE.
- ALL BOXES SHALL BE FORMED ON THE INSIDE AND OUTSIDE OF THE BOX AND INSPECTED BY THE CITY PRIOR TO THE PLACING OF CONCRETE.
- DOUBLE CATCH BASINS WILL BE REQUIRED IN LOCATIONS SPECIFIED BY THE CITY ENGINEER (TYPICALLY IN LOW SPOTS OR WHERE ADDITIONAL INLET CAPACITY IS NEEDED).
- STORM DRAIN LINES SHALL BE 15 INCH MINIMUM DIAMETER REINFORCED CONCRETE PIPE (RCP), OF APPROPRIATE CLASS.
- ALTERNATE STRUCTURE (E.G. COMBO BOXES) MAY BE USED WITH APPROVAL OF THE CITY ENGINEER. STRUCTURES SHALL FOLLOW APWA STANDARD PLANS AND BE A COMMON SIZE.

DOUBLE CATCH BASIN



BRANDON K. JONES  
No. 5148758  
PROJECT ENGINEER  
2-12-2019  
DATE

REV.	DATE	APPR.
1	JAN '19	BKJ
		ADDED NOTE

SCALE:  
N.T.S.

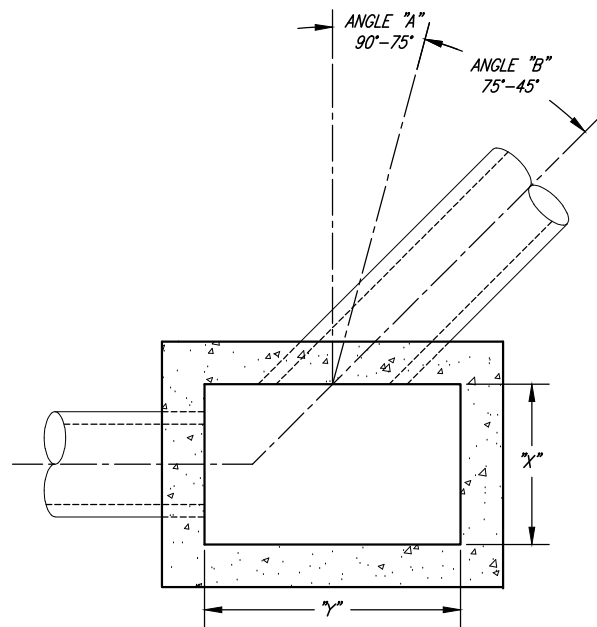
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STORM DRAIN - SINGLE AND DOUBLE CATCH BASIN DETAILS

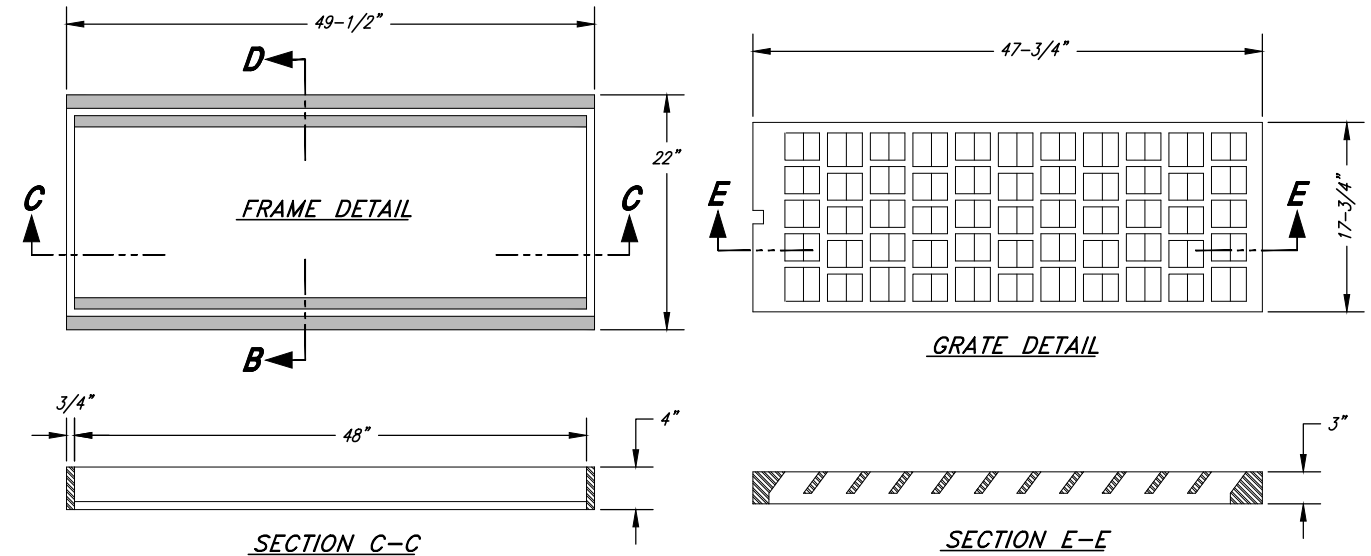
SHEET:  
CS-16  
OF 24 SHEETS  
0



PIPE SIZE (IN.)	"X"	INLET BOX		"Z" MIN.
		"Y" (ANGLE A)	"Y" (ANGLE B)	
15	2'-6"	4'-0"	4'-0"	2'-0"
18	2'-6"	4'-0"	4'-0"	2'-6"
21	4'-0"	4'-0"	4'-0"	3'-0"
24	4'-0"	4'-0"	5'-0"	3'-0"
30	4'-0"	4'-0"	6'-0"	3'-6"
36	4'-0"	4'-0"	6'-0"	4'-0"
42	6'-0"	6'-0"	7'-0"	5'-0"
48	6'-0"	6'-0"	8'-0"	5'-6"

**GENERAL NOTE:**

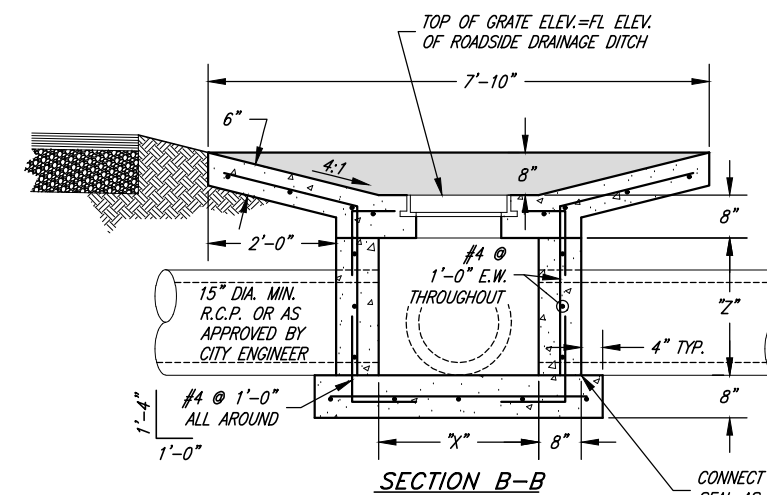
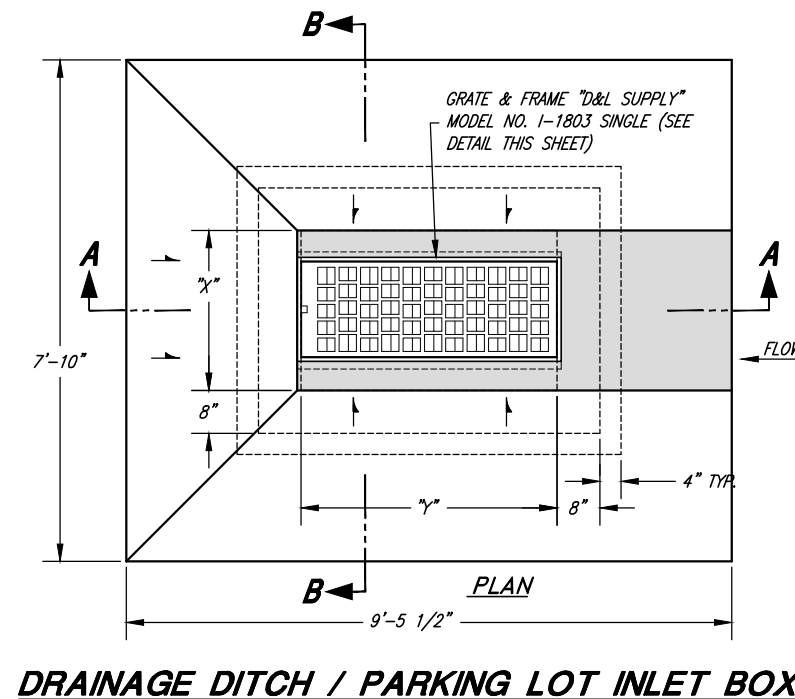
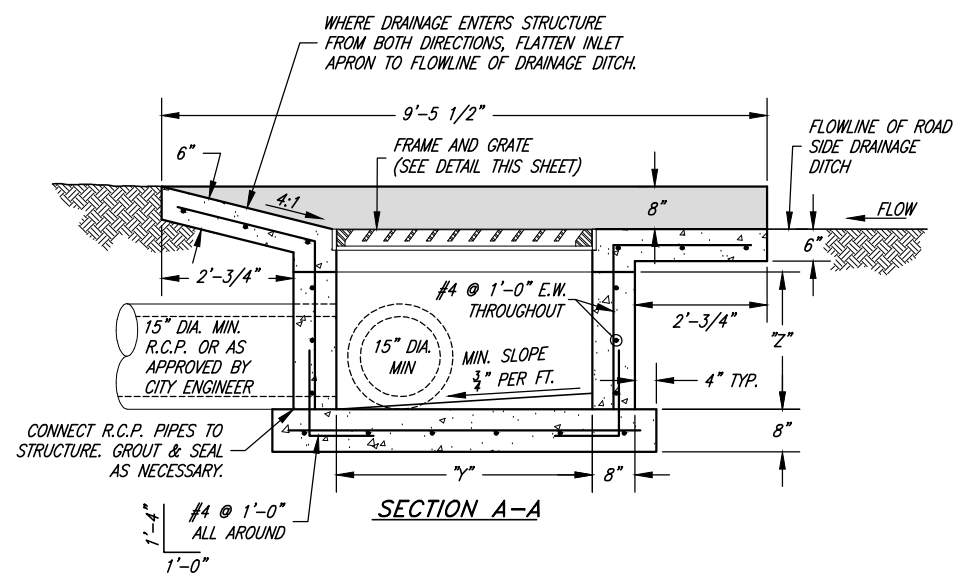
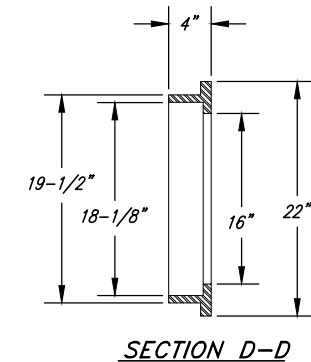
STORM DRAIN LINES SHALL BE 15 INCH MINIMUM DIAMETER REINFORCED CONCRETE PIPE (RCP), OF APPROPRIATE CLASS.



**FRAME & GRATE DETAILS**

**FRAME AND GRATE NOTES:**

- A1. GRATE AND FRAME SHALL BE AS MANUFACTURED BY "D&L SUPPLY" I-1803
- B1. BICYCLE SAFE GRATE REQUIRED.
- C1. "OR EQUAL" GRATES AND FRAMES WILL BE CONSIDERED AS APPROVED BY THE CITY ENGINEER.



**DRAINAGE BOX NOTES:**

1. ALL BOX SIZES REFLECT DIMENSIONS FOR THE MINIMUM 15" PIPE SIZE. BOX DIMENSIONS MUST INCREASE PROPORTIONALLY TO ACCOMMODATE LARGER PIPE SIZES. (SEE TABLE THIS SHEET)
2. CAST-IN-PLACE CONCRETE STRUCTURES CAN BE REPLACED WITH PRECAST CONCRETE STRUCTURES WITH HL-93 DECK LOADING AND COMPARABLE SIZE.
3. ALL BOXES SHALL BE FORMED ON THE INSIDE AND OUTSIDE OF THE BOX AND INSPECTED BY THE CITY PRIOR TO THE PLACING OF CONCRETE.



BRANDON KENT JONES  
No. 5148758  
PROJECT ENGINEER  
2-12-2019  
DATE

REV.	DATE	APPR.

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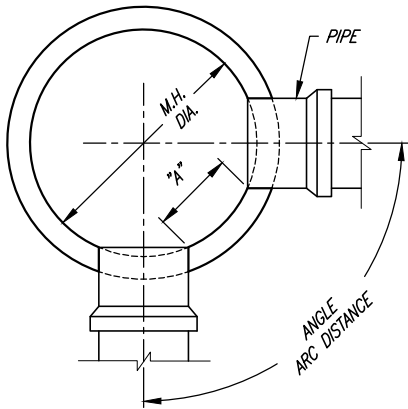


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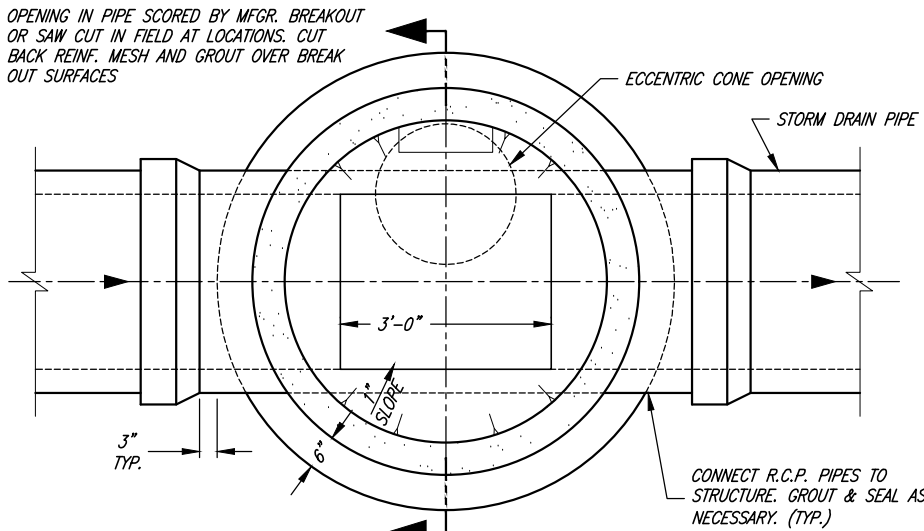
**SOUTH WEBER CITY CORPORATION**  
**PUBLIC WORKS STANDARDS**  
**STORM DRAIN - DRAINAGE INLET BOX & GENERAL**  
**GRATE AND FRAME DETAILS**

SHEET:  
**CS-17**  
OF 24 SHEETS  
0

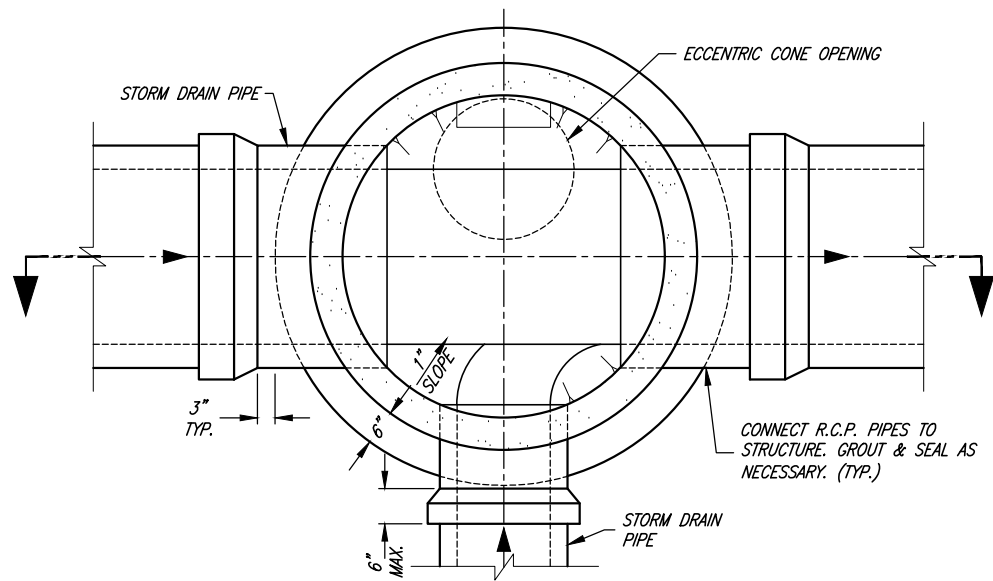
PIPE SIZES											
M.H. SIZE	IN-LINE M.H.	JUNCTION MANHOLE (ANGLE / ARC DISTANCE)									
	180"	90°	85°	80°	75°	70°	65°	60°	55°	50°	45°
4" M.H.	15"-24"	15"-18"	15"-18"	15"	15"	--	--	--	--	--	--
5" M.H.	27"-30"	21"-24"	21"-24"	18"-21"	18"-21"	15"-18"	15"-18"	15"	--	--	--
6" M.H.	36"-48"	27"-30"	27"-30"	24"-27"	24"	21"-24"	21"	18"	15"-18"	15"	--
7" M.H.	54"	36"	36"	30"	27"-30"	27"	24"	21"-24"	21"	18"	15"
8" M.H.	60"	42"	42"	36"	36"	30"	27"-30"	27"	24"	21"	18"



- SIZING NOTES:**
- SUGGESTED "A" DISTANCE IS 6" OR GREATER FOR 48", 60" AND 72" DIAMETER MANHOLES
  - SUGGESTED "A" DISTANCE IS 8" OR GREATER FOR 84" AND 96" DIAMETER MANHOLES



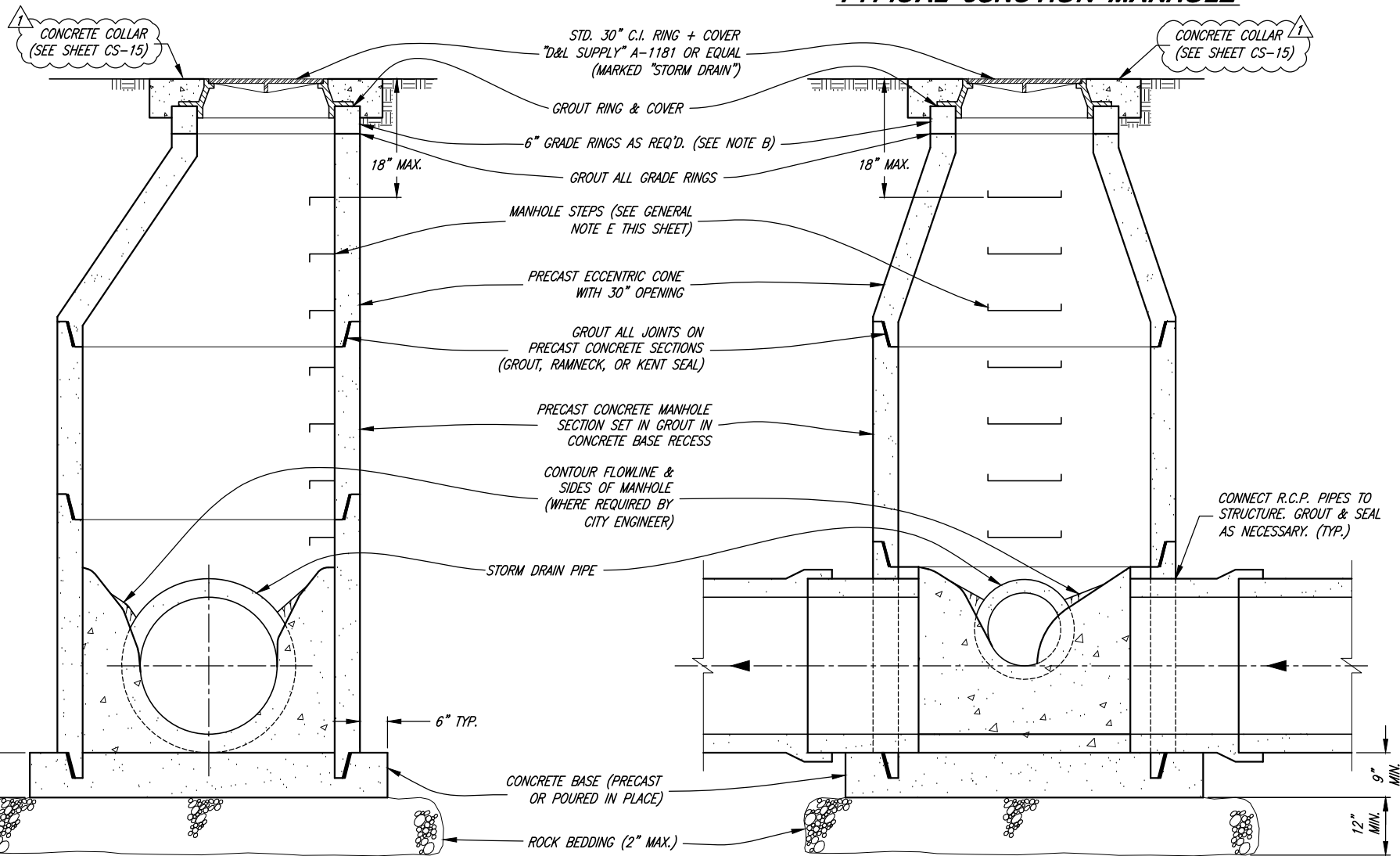
**TYPICAL LINE MANHOLE**



**TYPICAL JUNCTION MANHOLE**

**GENERAL NOTES:**

- STORM DRAIN MANHOLE DIAMETER TO BE DETERMINED BY THE DESIGN ENGINEER AFTER EVALUATION OF THE NUMBER, SIZE, AND PIPE ENTRY ANGLE OF THE PIPES THAT CONNECT TO THE MANHOLE.
- NO MORE THAN 12" OF GRADE RINGS TO BE ALLOWED ON ANY MANHOLE
- PLYWOOD COVERS SHALL BE USED AT MANHOLE FLOOR TO COVER FLOWLINE DURING CONSTRUCTION AND MAINTENANCE ACTIVITIES.
- ALL INTERIOR JOINTS SHALL BE SMOOTH AND EVENLY GROUTED WITH NON-SHRINK GROUT MIX.
- MANHOLE STEPS UNIFORMLY SPACED (1'-0" MAX.) POLYPROPYLENE COVERED STEEL STEPS, MODEL PSI-PF AS MANUFACTURED BY "M.A. INDUSTRIES" OR APPROVED EQUAL-INSTALLATION OF STEPS SHALL BE WATERPROOF.
- STORM DRAIN LINES SHALL BE 15 INCH MINIMUM DIAMETER REINFORCED CONCRETE PIPE (RCP), OF APPROPRIATE CLASS.
- FLAT MANHOLE RINGS & COVERS (SLAB CONSTRUCTION) ARE NOT ALLOWED ON ANY MANHOLE CONE SECTION.



BRANDON KENT JONES  
No. 5148758  
PROJECT ENGINEER  
2-12-2019  
DATE

REV.	DATE	APPR.	DESCRIPTION
1	JAN '19	BKJ	CONCRETE COLLAR-DELETED NOTE/DIMENSIONS; ADDED NOTES

SCALE:  
N. T.S.

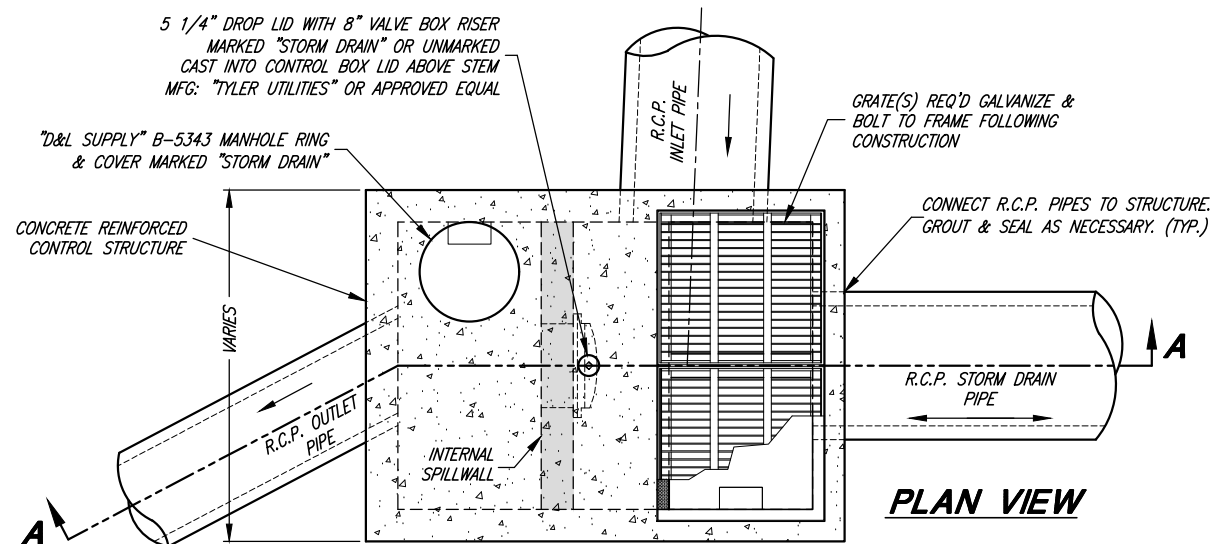
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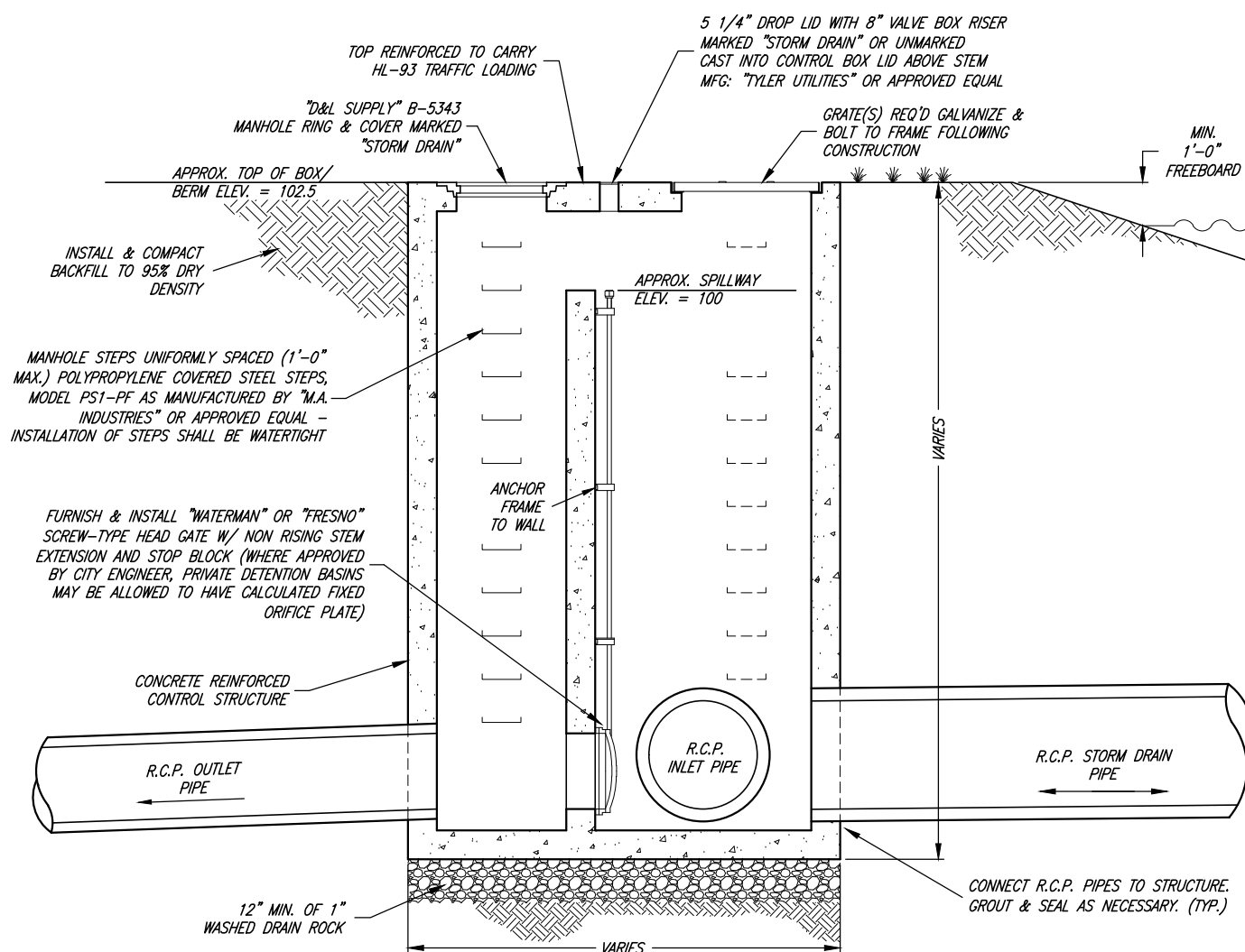
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**STORM DRAIN - MANHOLE DETAILS**

SHEET:  
**CS-18**  
OF 24 SHEETS  
0

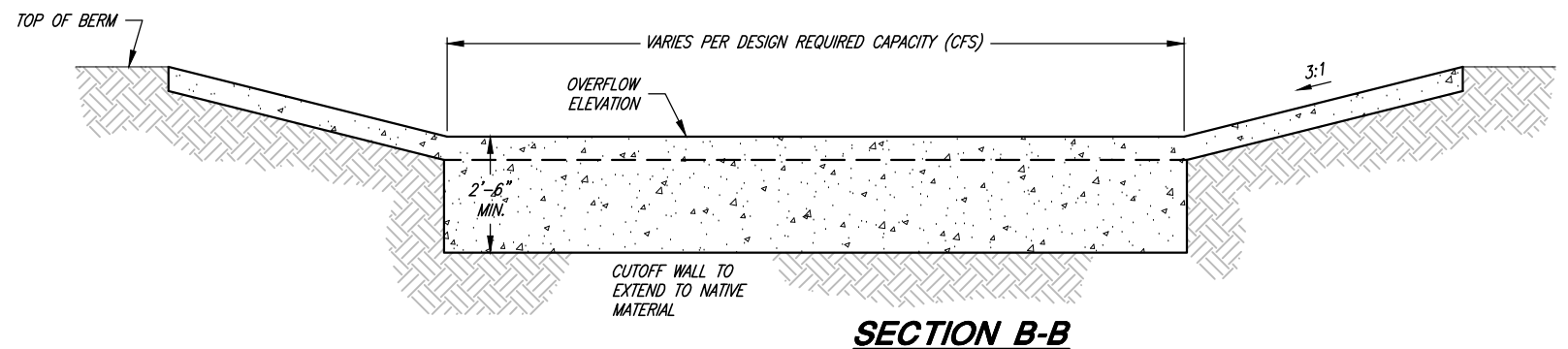


**DETENTION INLET/OUTLET CONTROL STRUCTURE**  
(PRECAST OR CAST-IN-PLACE)

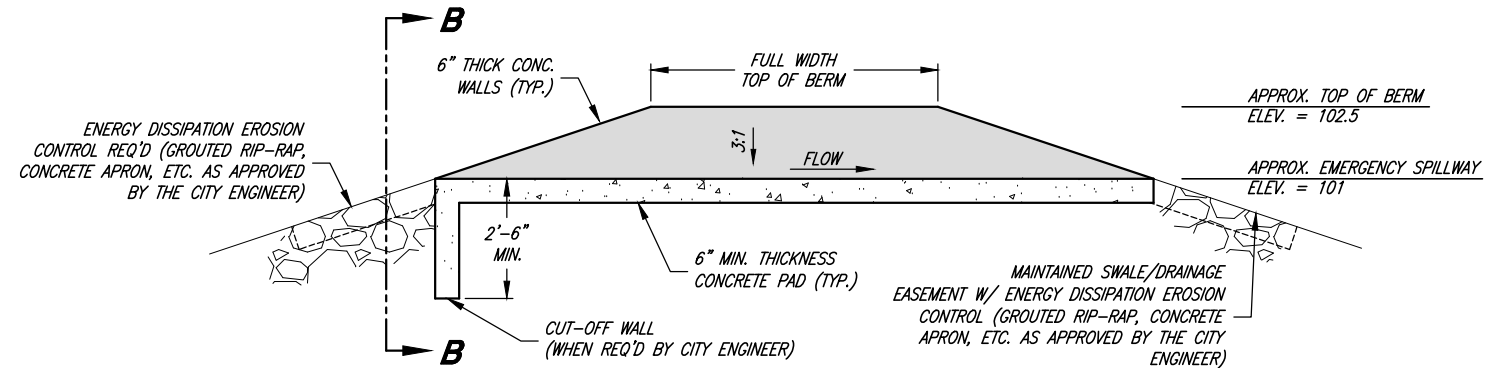


**SECTION A-A**

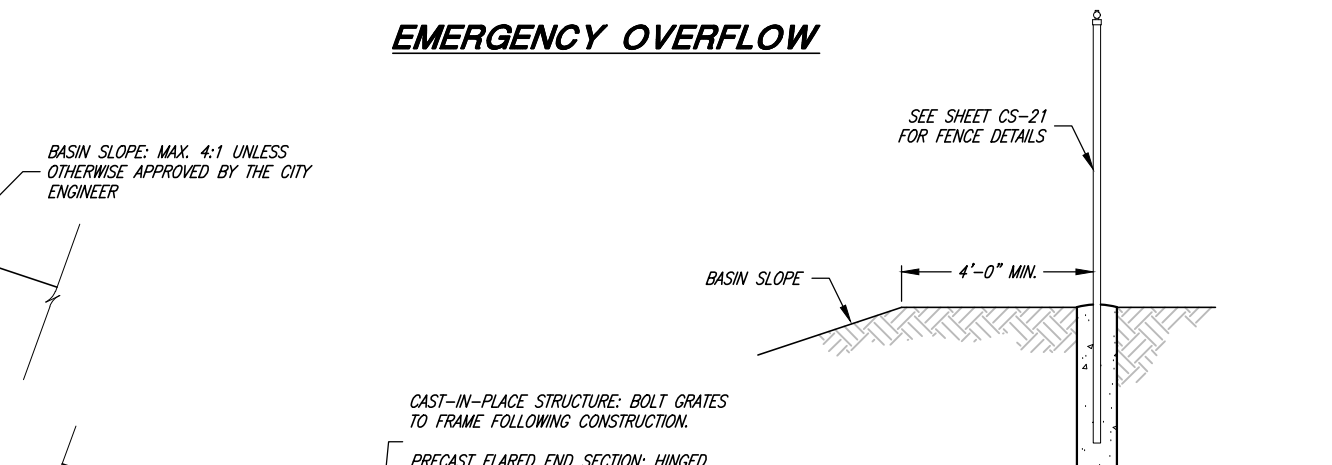
GENERAL AND STRUCTURAL NOTES:  
SEE SHEET CS-20



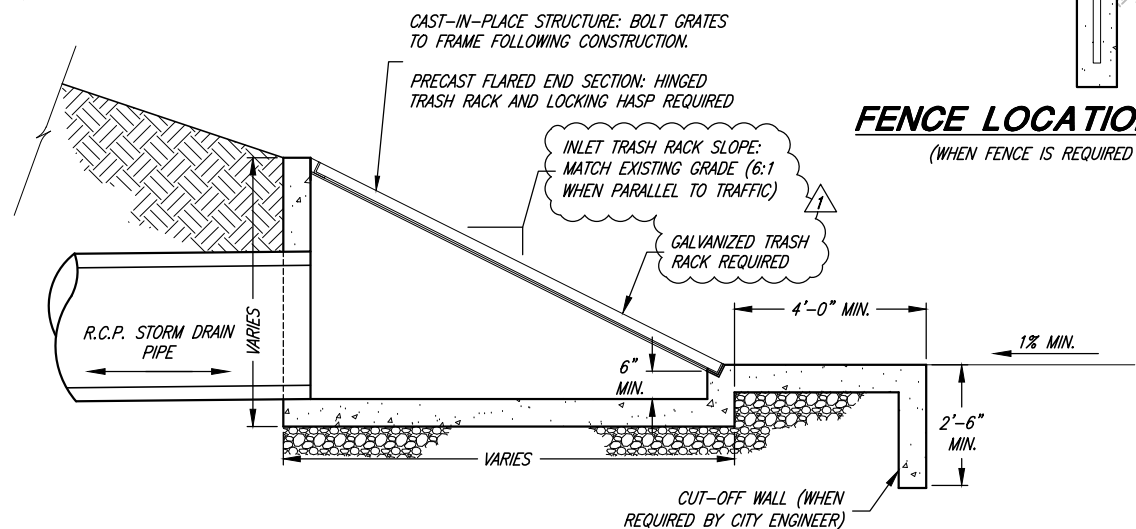
**SECTION B-B**



**EMERGENCY OVERFLOW**



**FENCE LOCATION DETAIL**  
(WHEN FENCE IS REQUIRED BY CITY)



**INCLINED GRATE STORM DRAIN INLET**



BRANDON KENT JONES  
No. 5148758  
PROJECT ENGINEER  
2-12-2019  
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REV.	DATE	APPR.	MODIFIED NOTES
1	JAN '19	BKJ	MODIFIED NOTES

SCALE:  
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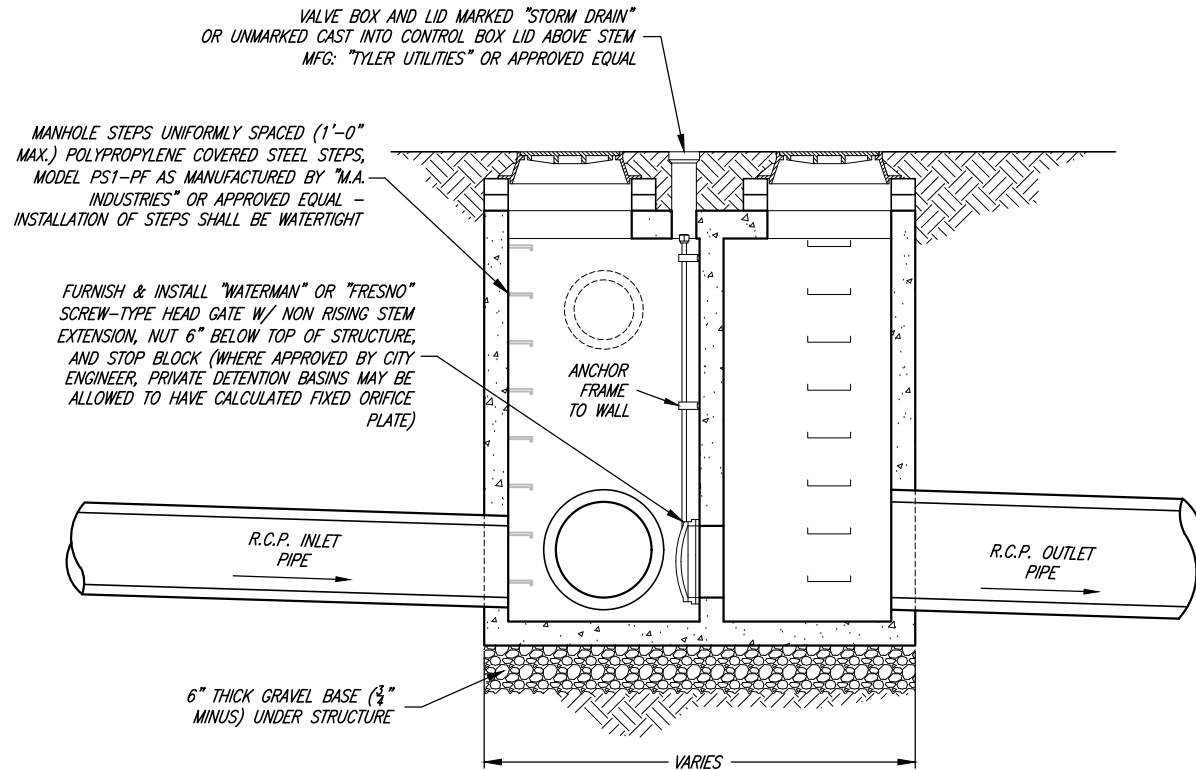


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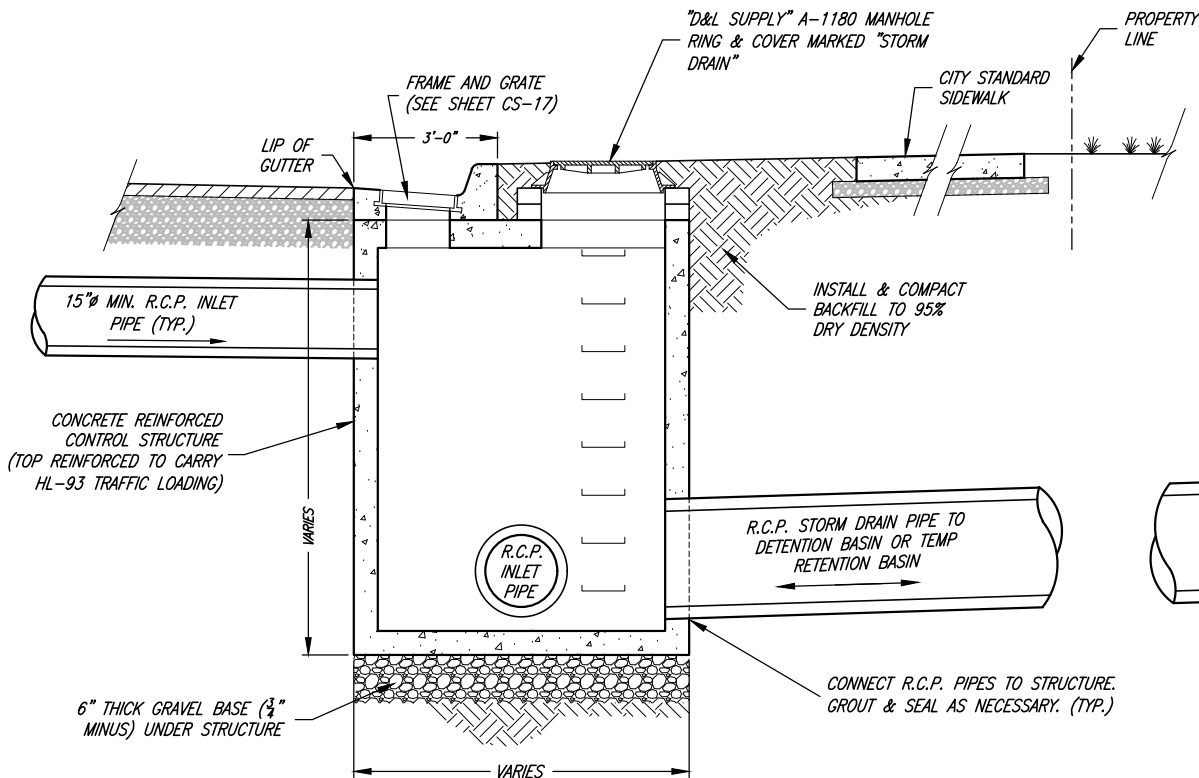
**SOUTH WEBER CITY CORPORATION**  
**PUBLIC WORKS STANDARDS**  
**STORM DRAIN - LARGE DETENTION BASIN DETAILS**

SHEET:  
**CS-19**  
OF 24 SHEETS  
0





**SECTION B-B**



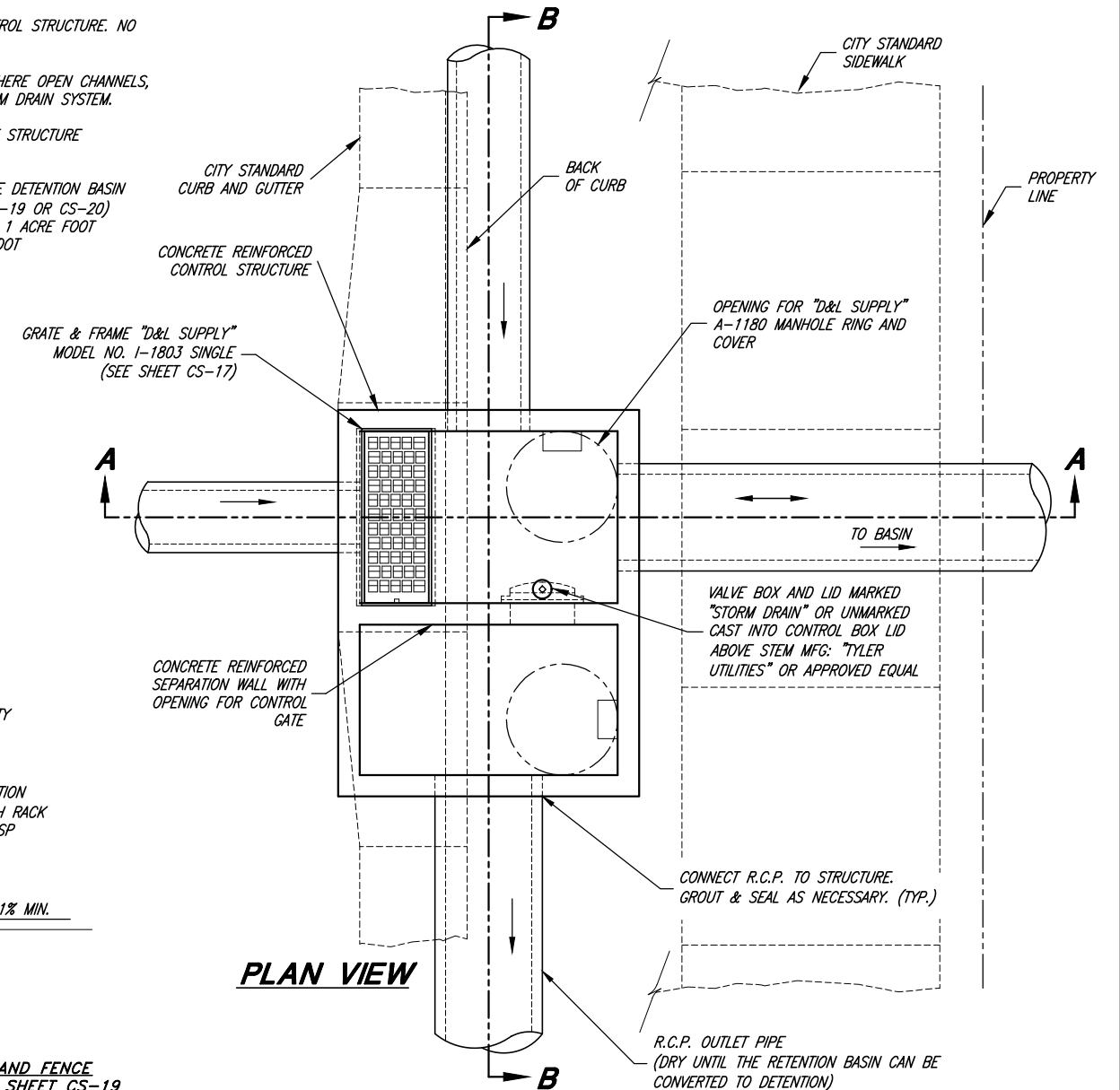
**SECTION A-A**

**GENERAL NOTES:**

1. ALL BASINS REGARDLESS OF LOCAL OR REGIONAL SHALL BE DESIGNED TO ACCOMMODATE A 100 YEAR STORM EVENT.
2. A DAM SAFETY (UTAH DIVISION OF WATER RIGHTS) HAZARD PERMIT MAY BE REQUIRED.
3. STRUCTURE DESIGN AND FLOW CALCULATIONS MUST BE APPROVED BY CITY ENGINEER PRIOR TO CONSTRUCTION.
4. STORM DRAIN LINES SHALL BE 15 INCH MINIMUM DIAMETER REINFORCED CONCRETE PIPE (RCP), OF APPROPRIATE CLASS.
5. THE SURFACE AREA OF THE BASIN SHALL BE SODDED AND SHALL BE PROVIDED WITH AN AUTOMATED SPRINKLER SYSTEM APPROVED BY THE CITY ENGINEER.
6. GRATES SHALL BE REMOVABLE FOR MAINTENANCE PURPOSES
7. GRATES SHALL BE HOT DIPPED GALVANIZED WITH BARS AT MAXIMUM 3 INCH SPACING.
8. LOW FLOWS MUST BE PIPED CONTINUOUSLY TO THE CONTROL STRUCTURE. NO OPEN FLOW IS PERMITTED THROUGH THE BASIN.
9. INCLINED GRATES ARE REQUIRED ON ALL PIPES/INLETS WHERE OPEN CHANNELS, DITCHES, OR PONDS DISCHARGE DIRECTLY INTO THE STORM DRAIN SYSTEM.
10. AN INTERNAL SPILLWAY MAY BE CONSTRUCTED INSIDE THE STRUCTURE DEPENDING ON SITE CONDITIONS AND ELEVATIONS.
11. BASIN STRUCTURES ARE DETERMINED BY THE SIZE OF THE DETENTION BASIN OR AS REQUIRED BY THE CITY ENGINEER. (SEE SHEET CS-19 OR CS-20)
  - a. SMALL DETENTION BASIN: LESS THAN OR EQUAL TO 1 ACRE FOOT
  - b. LARGE DETENTION BASIN: GREATER THAN 1 ACRE FOOT

**STRUCTURAL NOTES:**

- A. PRECAST CONCRETE STRUCTURE CAN BE REPLACED WITH CAST-IN-PLACE CONCRETE VAULT. SUBMIT ENGINEERED CONSTRUCTION PLANS WITH REBAR DETAILS TO CITY ENGINEER FOR REVIEW AND ACCEPTANCE PRIOR TO CONSTRUCTION.
- B. ADD REINFORCEMENT AROUND OPENINGS EQUAL TO REINFORCEMENT DISPLACED BY OPENING.
- C. THE PRECAST VAULT MANUFACTURER IS RESPONSIBLE FOR DESIGN RELATED TO TRAFFIC LOADING AND THRUST. VERIFICATION OF PROPER DESIGN MUST BE PROVIDED TO THE CITY BY THE DEVELOPER, CONTRACTOR, OR PROPERTY OWNER AS THE CASE MAY BE.
- D. REINFORCEMENT TO CONFORM WITH ASTM A 615 GRADE 60
- E. CONCRETE SHALL HAVE A 28-DAY COMPRESSIVE STRENGTH OF 5,000 PSI
- F. USE AN AIR-ENTRAINING AGENT ON ALL CONCRETE EXPOSED TO THE WEATHER.
- G. HL-93 LOADING



**PLAN VIEW**

**INLET/OUTLET CONTROL STRUCTURE**

(PRECAST OR CAST-IN-PLACE)



BRANDON KENT JONES  
No. 5148758  
PROJECT ENGINEER  
2-12-2019  
DATE

REV.	DATE	APPR.

SCALE:  
N.T.S.

DESIGNED BKJ  
DRAWN BEB  
CHECKED BKJ



**CONSULTING ENGINEERS**  
6080 Fashion Point Drive  
South Ogden, Utah 84403 (801) 476-9767  
www.jonescivil.com

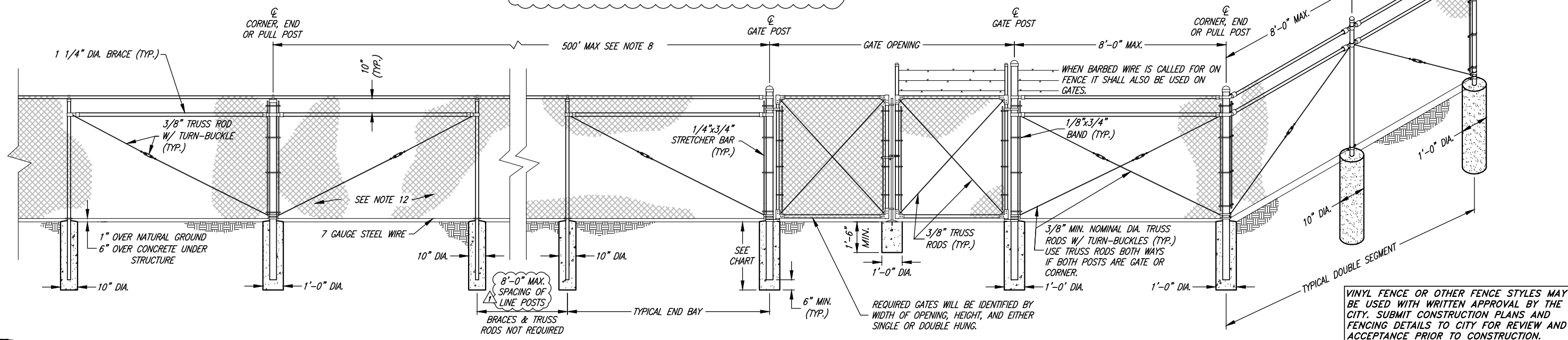
**SOUTH WEBER CITY CORPORATION**  
**PUBLIC WORKS STANDARDS**

**STORM DRAIN - SMALL DETENTION BASIN DETAILS**

SHEET:  
**CS-20**  
OF 24 SHEETS  
0



HEIGHT OF FABRIC	DEPTH OF POSTS	LENGTH OF END, CORNER OR PULL POST	LENGTH OF LINE POST	SIZE OF POSTS	
				END, CORNER, & PULL POSTS	LINE POST
					NOM. SIZE
7'	3'	10'	9'-8"	2 1/2"	2"
6'	3'	9'	8'-8"	2 1/2"	2"
5'	3'	8'	7'-8"	2"	1 1/2"
4'	3'	6'	5'-8"	2"	1 1/2"
3'	3'	5'	4'-8"	2"	1 1/2"



*Brandon K. Jura*  
PROJECT ENGINEER  
2-12-2019  
DATE

1	JAN '19	BKJ	ADDED DIMENSION; MODIFIED TABLE & NOTES.
REV	DATE	APP	

SCALE:

N.T.S.

DESIGNED BKJ  
DRAWN BEB  
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***SOUTH WEBER CITY CORPORATION***  
***PUBLIC WORKS STANDARDS***

### **GENERAL - CHAIN LINK FENCE DETAILS**

SHEET:

CS-21

OF 24 SHEETS

STREETLIGHT STYLES AND LOCATIONS

RESIDENTIAL (SL-1):

- FIXTURE STYLE: LAMP POST
- POLE HEIGHT: 14 FEET
- SPACING: MAXIMUM 400 FEET, ON ALTERNATING SIDES OF THE STREET
- LOCATION: CUL-DE-SACS, MID-BLOCK, AND MINOR INTERSECTIONS (WHEN APPROVED)

INTERSECTION (SL-2):

- FIXTURE STYLE: OVERHEAD WITH STRAIGHT DECORATIVE MAST ARM
- POLE HEIGHT: 18 FEET
- SPACING: INTERSECTIONS
- LOCATION: INTERSECTIONS AND PARKING LOTS (OR AS OTHERWISE DIRECTED)

CORRIDOR (SL-3):

- FIXTURE STYLE: OVERHEAD WITH CURVED DECORATIVE MAST ARM
- POLE HEIGHT: 18 FEET
- SPACING: MAXIMUM 300 FEET, ON ALTERNATING SIDES OF THE STREET
- LOCATION: ALONG THE SOUTH WEBER DRIVE AND SOUTH BENCH DRIVE CORRIDORS (OR AS OTHERWISE DIRECTED)
- ADDITION DECORATIVE FEATURES: BANNER POLES, FLOWER BASKETS, ETC. (AS APPROVED)

STREETLIGHT REQUIREMENTS

POLES:

1. ALL POLES MUST BE SEMI-GLOSS BLACK
2. ALL POLES MUST BE THE SAME DIAMETER (NON-TAPERED), BUT HEIGHT VARIES AS STATED ABOVE
3. ALL POLES MUST BE MOUNTED TO A CONCRETE FOOTING PER MANUFACTURER RECOMMENDATIONS
4. ANCHOR BOLTS MUST NOT CONFLICT WITH THE BASE
5. INTERSECTION AND CORRIDOR POLES MUST HAVE A GFCI LOCATED 2 FEET BELOW THE TOP OF THE POLE

BASES:

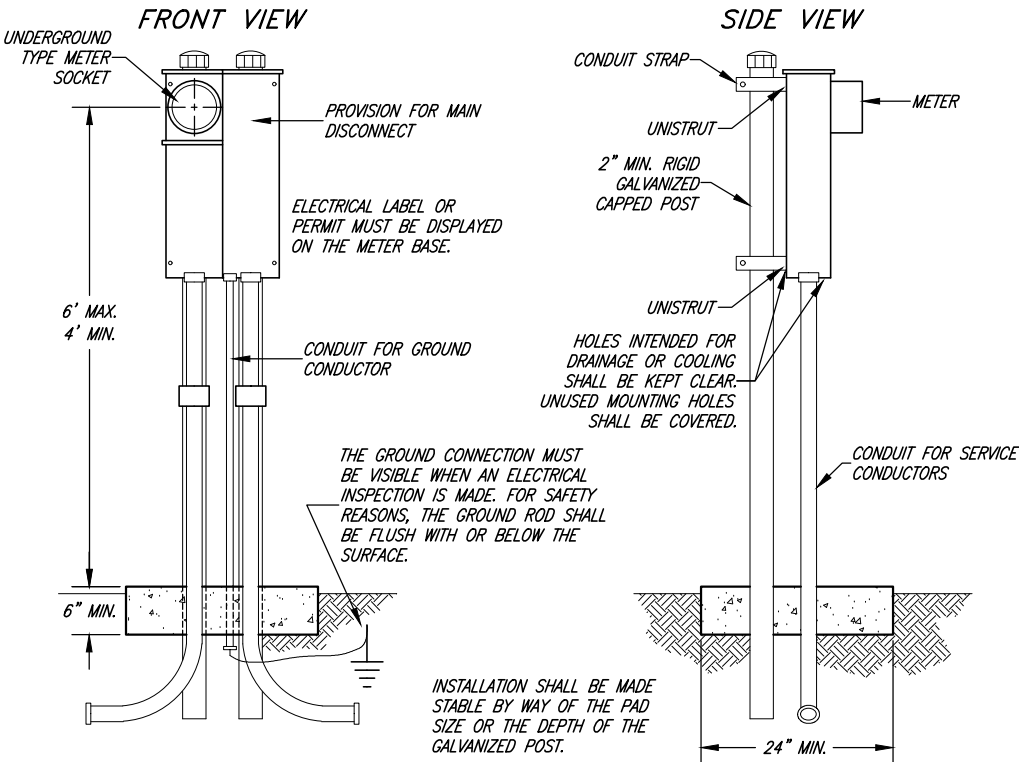
6. ALL BASES MUST BE SEMI-GLOSS BLACK
7. ALL BASES MUST BE THE SAME STYLE AND SIZE
8. ALL BASES MUST BE TWO-PIECE CAST ALUMINUM
9. ALL BASES MUST CONTAIN THE CITY NAME IN SANDED ALUMINUM TEXT, AS APPROVED BY THE CITY
10. ALL BASES MUST HAVE A HANDHOLE WITH COVER

LIGHT FIXTURE:

11. ALL STREETLIGHT FIXTURES MUST BE LED
12. ALL STREETLIGHT FIXTURES MUST BE FULLY DARK-SKY COMPLIANT

GENERAL NOTES:

13. THE COST OF ALL NEW DEVELOPMENT STREETLIGHTS IS THE RESPONSIBILITY OF THE DEVELOPER, AND IS PAID DIRECTLY TO THE CITY, PRIOR TO THE RECORDING OF THE PLAT
14. THE CITY IS RESPONSIBLE FOR INSTALLATION OF ALL STREETLIGHTS
15. THE DEVELOPER IS RESPONSIBLE TO PROVIDE ALL POWER INFRASTRUCTURE FOR THE DEVELOPMENT, INCLUDING COORDINATION WITH THE POWER COMPANY FOR CONNECTION AND SERVICE TO THE PROPOSED STREETLIGHTS
16. ALL PROPOSED STREETLIGHT TYPES AND LOCATIONS MUST BE SHOWN ON THE APPROVED IMPROVEMENT PLANS
17. THE JUNCTION BOX MUST BE FLUSH TO GRADE AND LOCATED WITHIN A MINIMUM OF 4' AND MAXIMUM OF 10' FROM THE BASE OF THE POLE
18. FOR SAFETY PURPOSES, STREETLIGHTS MUST CONTAIN BREAK-AWAY STYLE FEATURES
19. ALL STREETLIGHTS MUST BE DESIGNED TO MEET ALL BUILDING CODE STRUCTURAL REQUIREMENTS
20. ALL STREETLIGHTS SHOULD BE LOCATED ON LOT LINES WHEN NOT LOCATED AT AN INTERSECTION
21. ALL STREETLIGHTS SHOULD BE LOCATED 2.5 FEET BEHIND THE BACK OF CURB OR BACK OF SIDEWALK



UNDERGROUND SERVICE TO A FREE-STANDING METER BASE

(STEEL POLE)  
INSTALLATION PER ROCKY MOUNTAIN POWER  
ELECTRIC SERVICE REQUIREMENTS MANUAL

DEVELOPER/CONTRACTOR WILL FURNISH AND INSTALL:

- A. METER SOCKET ENCLOSURE (UNDERGROUND TYPE WITH MANUAL-LINK BYPASS)
- B. PEDESTAL HARDWARE
- C. CONDUIT
- D. RIGHT-OF-WAY OR EASEMENT
- E. TRENCH EXCAVATION AND BACKFILL
- F. GROUNDING PER NEC
- G. CONCRETE PAD 24" x 24" x 6" DEPTH
- H. LONG RADIUS SWEEP
- I. 36" SWEEP

FREE-STANDING METER BASE REQUIREMENTS:

- A1. THE DEVELOPER/CONTRACTOR SHALL MEET WITH THE POWER COMPANY TO DETERMINE THE LOCATION OF THE FREE-STANDING METER BASE.
- B1. THE FREE-STANDING METER BASE SHALL BE LOCATED ADJACENT TO, OR IN, THE POWER COMPANY EASEMENT.
- C1. THE FREE-STANDING METER BASE SHALL MEET ALL LOCAL ORDINANCE REQUIREMENTS.
- D1. THE METER SOCKET SHALL BE PROTECTED FROM DAMAGE BY USE OF BARRIER POSTS OR OTHER SUITABLE PROTECTION APPROVED BY THE POWER COMPANY.
- E1. THE DEVELOPER/CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN AN APPROVED PEDESTAL OR POLE POST.
- F1. THE ACCESS DOOR TO POWER COMPANY CONNECTIONS SHALL BE KEPT FREE OF OBSTRUCTIONS A MINIMUM OF 6" ABOVE THE FINAL GRADE, WITH A SEALABLE PROVISION FOR THE POWER COMPANY.
- G1. THE UNMETERED SERVICE CONDUCTOR AND THE METERED SERVICE CONDUCTOR SHALL NO BE RUN IN THE SAME CONDUIT, RACEWAY, OR GUTTER.
- H1. THE METER SOCKET AND SERVICE EQUIPMENT SHALL BE NEMA TYPE 3R (RAINPROOF), IN GOOD CONDITION WITH NO HOLES, DENTS OR DAMAGE, AND PLUMB IN ALL DIRECTIONS. THE INSTALLATION SHALL BE MADE WITH SUFFICIENT MATERIALS AND INSTALLED SUCH THAT IT REMAINS PLUMB FOR THE DURATION OF THE SERVICE.
- I1. CONDUIT AND CONDUCTOR TRENCHERS SHALL BE LOCATED AWAY FROM (AND NEVER UNDERNEATH) THE PAD AND FOUNDATION. FOR MOBILE HOMES, TRENCHES SHALL BE LOCATED CLEAR OF THE AREA PROVIDED FOR THE DWELLING.
- J1. WHERE TWO OR MORE METERS ARE LOCATED SIDE-BY-SIDE (SUCH AS WITH DUPLEXES OR IN MOBILE HOME PARKS), THE METER SOCKET ENCLOSURE SHALL BE PERMANENTLY LABELED WITH THE SPACE OR BERTH NUMBERS.



PROJECT ENGINEER  
2-12-2019  
DATE

1	JAN '19	BKJ	DELETED EX. LIGHT DETAILS; UPDATED STREETLIGHT NOTES.
REV.	DATE	APPR.	

SCALE:

N. T.S.

DESIGNED BKJ  
DRAWN BEB  
CHECKED BKJ

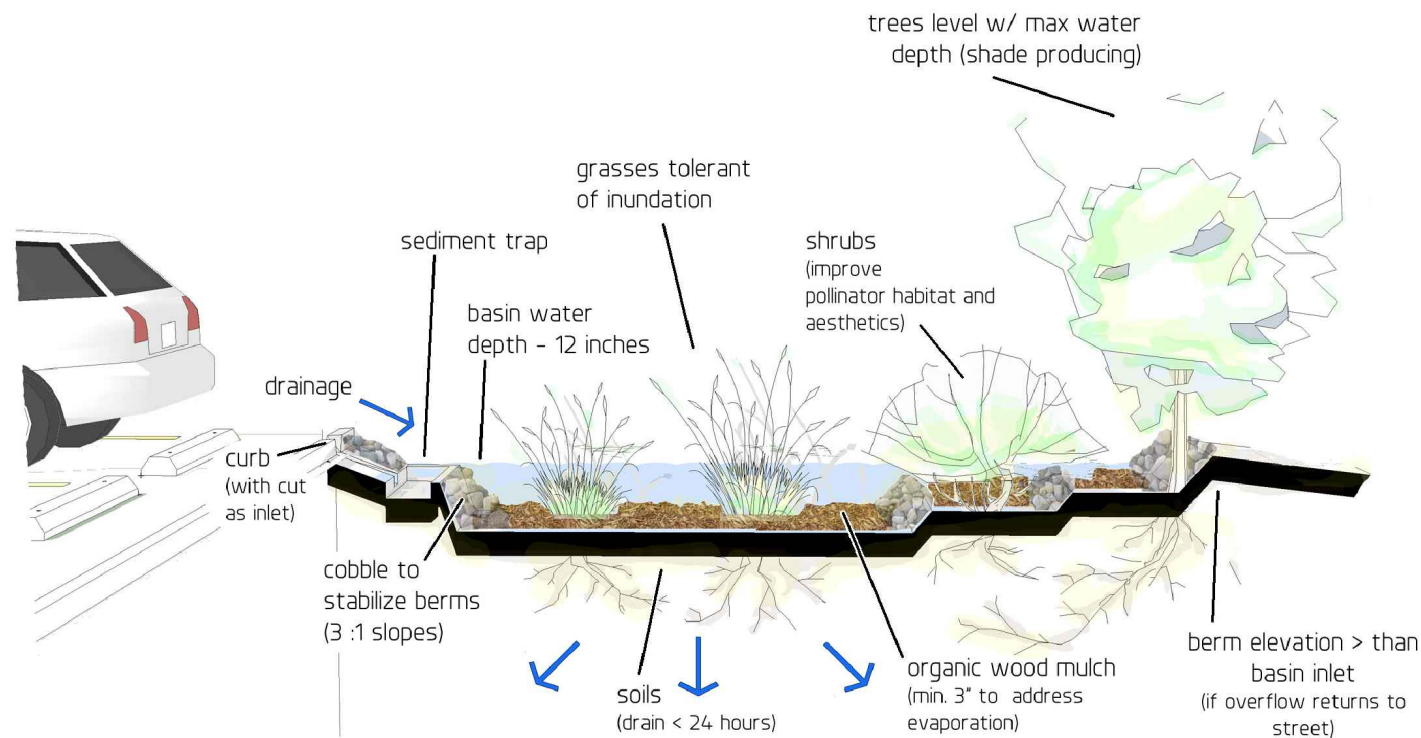


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South Ogden, Utah 84403 (801) 476-9767  
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GENERAL - STREET LIGHTING STANDARDS

SHEET:  
CS-22  
OF 24 SHEETS  
0





## Basic Basin Design Considerations

### RAIN GARDEN

\*\*\* [http://www.lid-stormwater.net/site\\_map.htm](http://www.lid-stormwater.net/site_map.htm) \*\*\*

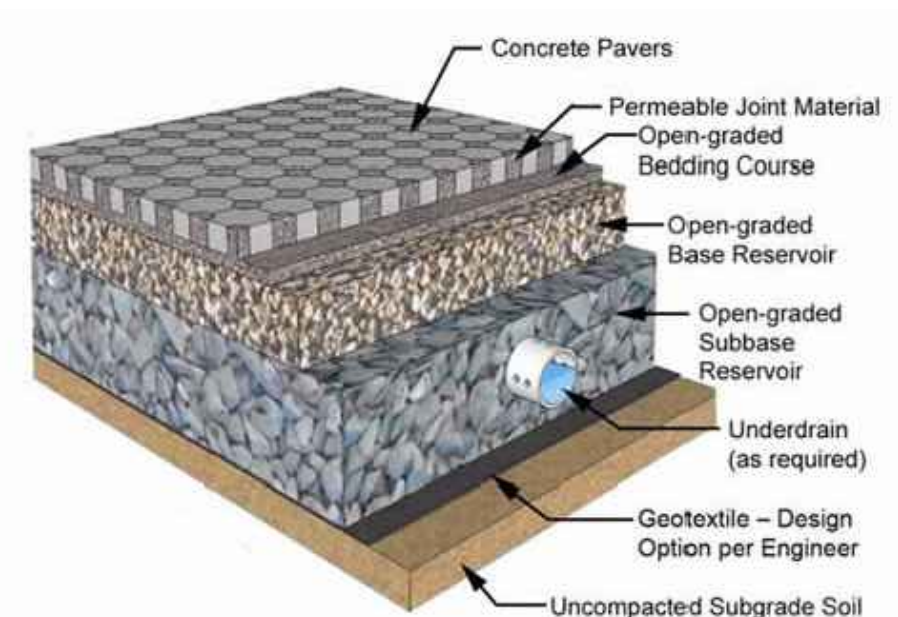
diagram by  
Paul Navrot  
for SUH



### RAIN BARREL

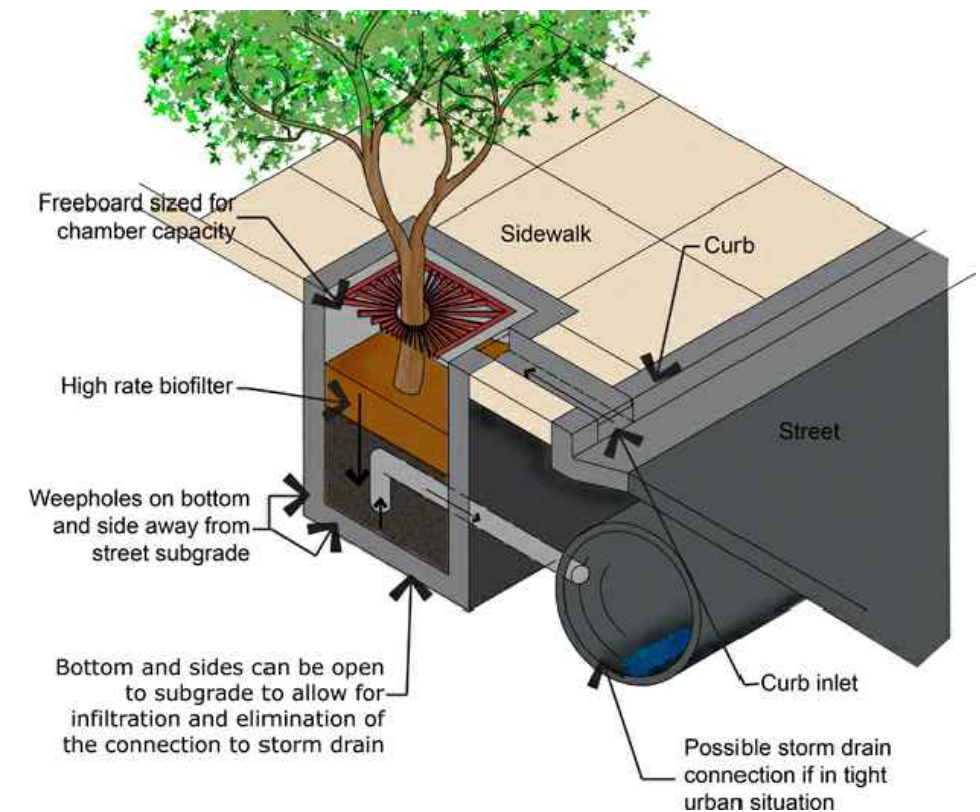
\*\*\* <http://www.goodideasinc.com/products/rain-barrels/rain-wizard-50/> \*\*\*

**DISCLAIMER:**  
ALL LID EXAMPLES SHOWN ON THIS SHEET ARE FOR REFERENCE PURPOSES ONLY. ANY SPECIFIC WEBSITES, COMMERCIAL PRODUCTS, PROCESS OR SERVICE BY TRADE NAME, TRADEMARK, MANUFACTURER, OR OTHERWISE, DOES NOT CONSTITUTE OR IMPLY ITS ENDORSEMENT, RECOMMENDATION, OR FAVORING BY SOUTH WEBER CITY. THE PURPOSE OF PROVIDING SPECIFIC PRODUCT INFORMATION IS TO ENSURE THAT THE CONTRACTOR AND/OR DEVELOPER HAS ALL THE APPROPRIATE INFORMATION AND REFERENCES TO ASSESS THE USEFULNESS OF THE PRODUCT.



### PERMEABLE PAVER

From Smith, D. 2006. *Permeable Interlocking Concrete Pavement—selection design, construction and maintenance. Third Edition.* Interlocking Concrete Pavement Institute. Herndon, VA



### TREE BOX FILTER

From [www.wbdg.org](http://www.wbdg.org)



BRANDON KENT JONES  
No. 5148758  
PROJECT ENGINEER  
2-12-2019  
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GENERAL - LID (LOW IMPACT DEVELOPMENT) EXAMPLES

SHEET:  
CS-23  
OF 24 SHEETS  
0